

Burquitlam–Lougheed Neighbourhood Plan *Streetscape Guidelines*

*Shaping growth in
a neighbourhood of
neighbourhoods*



This document provides guidelines for development within the Burquitlam - Lougheed Neighbourhood Plan area which are for general guidance only. Site specific conditions will apply and drawings will be assessed by Development Services staff for adherence, completeness and alignment with plan and guideline intent.

All materials and construction shall conform to the platinum edition of the master municipal constructions documents (MMCD) and MMCD supplementary updates to date and City of Coquitlam supplementary specifications and detail drawings unless otherwise noted.



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EXECUTIVE SUMMARY

This update to the Burquitlam – Lougheed *Streetscape Guidelines* has been prepared to detail the improvements to the overall look and feel of key public areas in Burquitlam – Lougheed. These improvements include on-street parking, improved sidewalks, street furniture, public walkways and plazas, public art, street trees and landscaping.

Design objectives of the Burquitlam – Lougheed Neighbourhood Plan were used to develop specific design guidelines for key areas of Burquitlam – Lougheed.

These include:

- › Strong design elements that identify the Burquitlam – Lougheed Neighbourhood and unify the sense of place within the community with aesthetically attractive art, street furnishing, building design and street frontage / greenway design. They provide a type of neighbourhood ‘branding’ with a unique design vocabulary.
- › Streetscape Design Standards and Zones to ensure that the flow of movement is not impeded by furnishing, that building frontages interact with the street without affecting movement, and ensure smooth transition from sidewalk to crosswalk.
- › Greenway Design Guidelines to guide the development and form of the neighbourhoods’ Citywide Greenways and Neighbourhood Greenways improving the overall pedestrian and cycling experience.
- › Node design guidelines that shape future focal points and gathering points in the neighbourhood. The Node design guidelines will ensure that nodes are prominent and usable natural meeting and resting places throughout the plan area.
- › Site Furnishing Requirements to ensure that as the neighbourhood’s streetscapes are improved, the furnishings, paving materials, lighting, landscaping and public art fit existing neighbourhood character, and build a stronger and more resilient streetscape.
- › The majority of streetscape improvements will be constructed at the time of redevelopment and funded by the developer. A staff interdepartmental working group determined the appropriate scope and level of design that will be required to improve streetscapes in the BLNP.

These *Streetscape Guidelines* will help ensure high-quality urban design, function, and a cohesive, fiscally balanced program.

1.0 INTRODUCTION

1.1 INTENT

The BLNP Streetscape Guidelines are a companion document to the *Burquitlam – Lougheed Neighbourhood Plan* (BLNP) as adopted by Coquitlam City Council in 2017. The BLNP forms part of the City of Coquitlam’s *Citywide Official Community Plan* (CWOCP). These guidelines are to be used as part of the design and review of public and private development projects within the lands delineated in *Schedule A* of the BLNP.

These guidelines address the design of the following:

- › Streetscapes,
- › Greenways, and
- › Public Open Space.

The guidelines of this document provide specificity to the Vision and policies of the updated BLNP and focus on three critical elements:

- › Creation of neighbourhood identity,
- › Achievement of livability, and
- › Sense of a human scale within a pedestrian-friendly environment.

Except as noted in this document, areas outside the boundaries of where these streetscape guidelines apply are guided by the standard *City of Coquitlam’s Subdivision and Development Servicing Bylaw No. 3558, 2003*, and other City standards and requirements.



Artist's rendering of a Burquitlam – Lougheed streetscape



1.2 HOW TO USE THESE GUIDELINES

These guidelines are to be used in the design and review of works and services (known as frontage improvements) for streetscapes in key areas of Burquitlam – Lougheed. These guidelines supplement street design standards in the City of Coquitlam *Subdivision and Development Servicing Bylaw*. When beginning a design or review process, use the following progression:

- › **Step 1:** Review the Streetscape Design Principles (Section 2.0) to ensure streetscape designs conform to the goal of achieving great streetscapes in Burquitlam – Lougheed.
- › **Step 2:** Use **Figure 3** to determine whether a subject property is along a section of street with a dedicated streetscape level, refer to **Figure 9** to determine whether the property is along a Greenway, and finally review **Figure 10** to determine if the subject property is identified as a **node, gateway or landmark site**.
- › **Step 3:** Use **Matrix 1 Streetscape Furnishings Palette** to determine furnishing materials and section **4.0 Site Furnishings** for instructions regarding furnishings - including colour, street trees and landscaping, lighting requirements and public art.

Evidence shows that people have better health when they live in communities that are designed to support day-to-day healthy choices, such as being physically active, eating healthy food, and engaging in positive social interactions.

From planh.ca

It is possible for applicants/developers to propose streetscape elements beyond the minimum provisions set out for each frontage type, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the Director of Development Services.

Should a conflict exist between the application of the Streetscape elements identified in this document and typical City of Coquitlam standards, these guidelines take precedence.

1.3 GUIDING PRINCIPLES & BEST PRACTICE

These *Streetscape Guidelines* support the Burquitlam – Lougheed Neighbourhood community in the following ways:

- › Create streetscapes that foster a sense of place and neighbourhood identity with improved urban design, enhancing the look and feel of the area.
- › Create streets that contribute to the economic success of local businesses and to the social well-being of residents and revitalization.
- › Create streetscapes that maximize the quality of experience, comfort, safety and universal access for pedestrians while maintaining balance between all modal users.
- › Create streetscapes that have long-lasting durability, timelessness, and as such are easy to maintain.
- › Create quality, universally accessible streetscapes that maximize experience with increased comfort, safety, and function while accommodating all modal users.

Cultures and climates differ all over the world, but people are the same. They'll gather in public if you give them a good place to do it.

Jan Gehl

1.4 DEFINITION OF STREETScape ZONES

For the purpose of these Guidelines, the streetscape is defined as the area in the road right-of-way between the curb (next to vehicle travel lanes) and the property line (or building face where the streetscape treatment extends onto private property).

More specifically, the streetscape consists of four zones that serve different functions as illustrated in Figures 1 and 2:

- **Curb Zone**
- **Furnishing or Boulevard Zone** (as determined by adjacent land use);
- **Pedestrian Movement Zone**; and
- **Building Frontage Zone**.

1.4.1 Curb Zone

The **Curb Zone** includes barrier or rollover curbs, letdowns for pedestrian and vehicle traffic and gutters to convey rain water to drains and rain gardens where applicable. This zone also accommodates parking pockets in commercial areas.

Figure 1: Streetscape Zones: Commercial or High Density Residential Uses

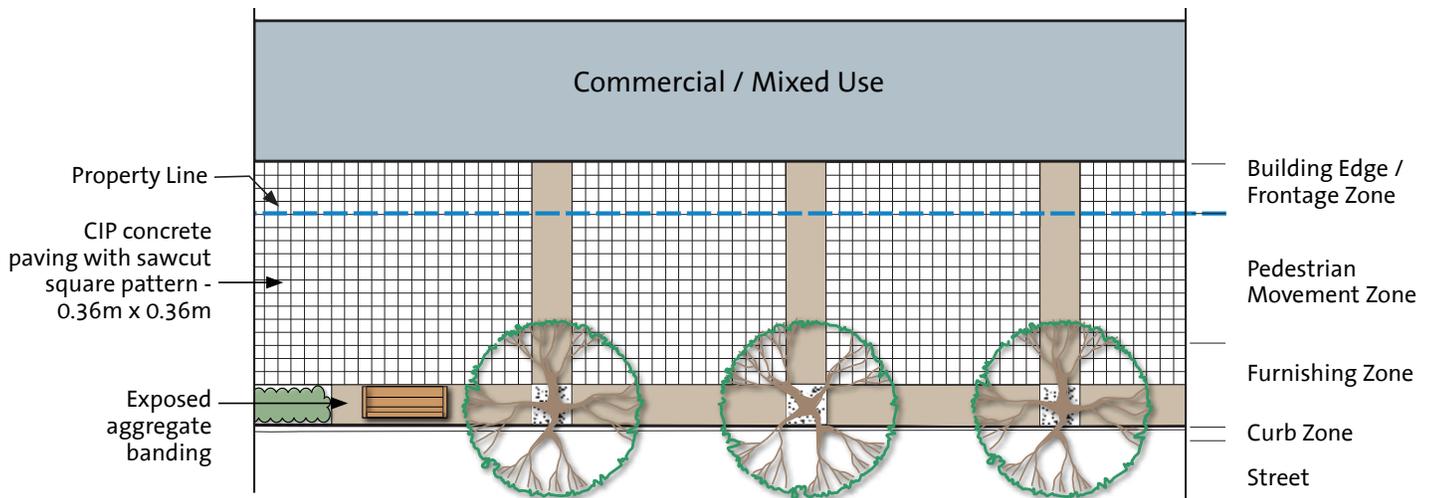
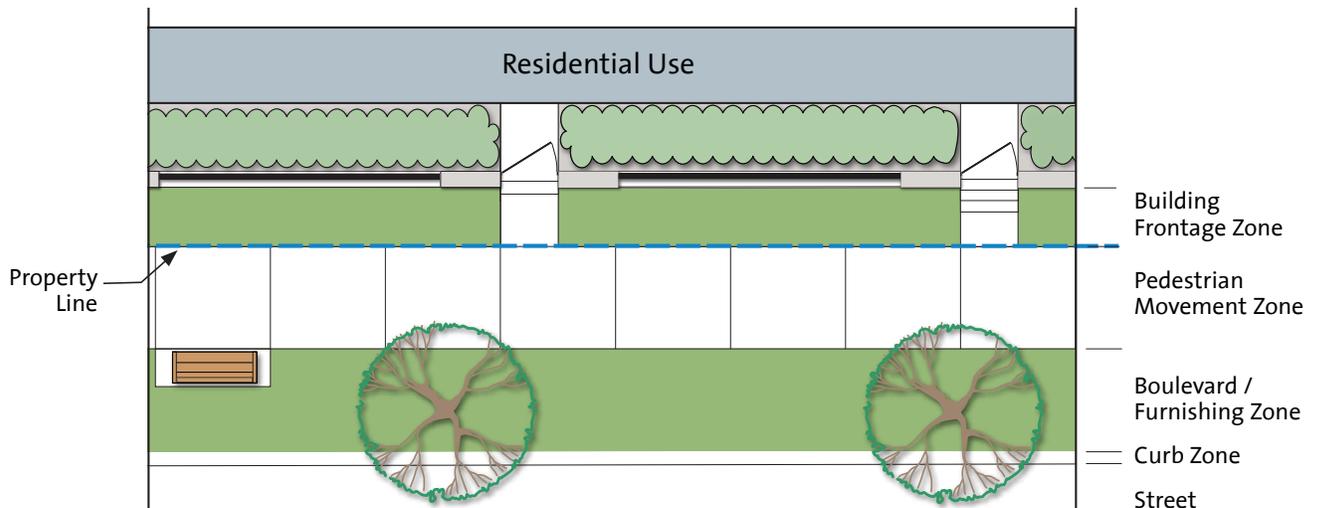


Figure 2: Streetscape Zones: Lower Density Residential Uses





1.4.2.a Furnishing Zone

(Applies to Commercial or Higher Density Residential Streetscapes)

The **Furnishing Zone** is the hard-surfaced area of concrete or pavers adjacent to and between the **Curb Zone** and the **Pedestrian Movement Zone**. It buffers pedestrians from the adjacent roadway and is where streetscape amenities, such as benches, bike racks, streetlights, street trees, landscaping and other elements that could be an obstacle to pedestrian movement are located. Physically separating pedestrians from vehicle travel lanes greatly increases their comfort and this buffering function is especially important on streets where vehicular traffic is heavy, traffic speeds are higher or where on-street parking does not exist.

1.4.2.b Boulevard Zone

(Applies to medium or low density residential Streetscapes)

In areas of medium or low density residential development, the zone between the **Curb Zone** and the **Pedestrian Movement Zone** is called the **Boulevard Zone**. This strip is adjacent to the curb that buffers pedestrians from the adjacent roadway. In this setting it is typically planted with grass or planting but continues to contain the various street elements that would otherwise impede pedestrian movement. In some instances it can include paving and site furniture such as bike racks or seating.

1.4.3 Pedestrian Movement Zone

The **Pedestrian Movement Zone** or “sidewalk” is the area intended for universally accessible pedestrian travel and is free of obstacles, continuous, uniform and predictably located between the **Furnishing / Boulevard Zone** and the **Building Frontage Zone**. This zone also supports universal access, including the use of wheelchairs and strollers. The minimum clearance in the **Pedestrian Movement Zone** should be 2.0 metres.

1.4.4 Building Frontage Zone

The **Building Frontage Zone** is the area between the **Pedestrian Movement Zone** and the property line or building face. To promote an active and vibrant street frontage a variety of elements, such as litter receptacles, planter boxes, merchandise displays and café seating, are encouraged within this zone. Vibrancy can be further enhanced where commercial uses such as cafés open onto the street with garage-door style access.

1.4.5 Adjoining Streetscapes

Where the street right-of-way is located along the boundaries of a Burquitlam – Lougheed neighbourhood area identified in this document, the opposite side of the street, outside of the Neighbourhood Plan Area, should adopt the higher standard streetscape character as defined by these guidelines. This is to ensure a contiguous streetscape aesthetic and avoid differences from one side of a street to the other.

1.4.6 Laneways

Laneways provide critical access for high-density development, but can also offer additional canvas for passive streetscaping. Where appropriate, development should be encouraged to include some laneway programming to complement the neighbourhood character.

*The minimum clearance between street furniture and infrastructure in the **Pedestrian Movement Zone** will be 2.0 metres, to allow two people to walk or wheel side by side.*

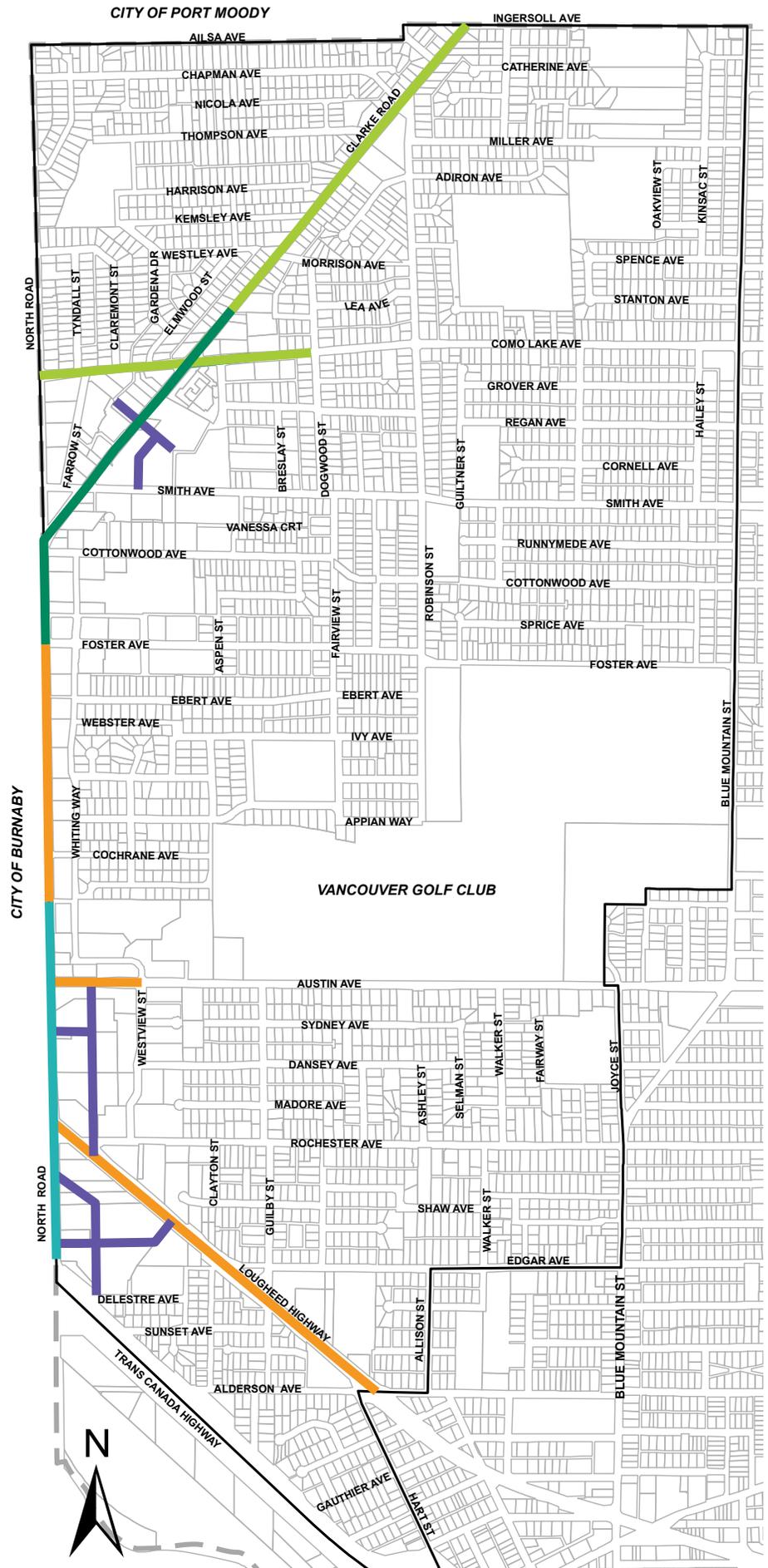
Figure 3: BLPN Streetscape Hierarchy

Legend

-  Level 1A - North Road - Commercial Frontage
-  Level 1B - Clarke Road - Commercial Frontage
-  Level 2A - North Road (et al) - Residential Frontage
-  Level 2B - Clarke Road (et al) - Residential Frontage
-  Level 3 - Peripheral Commercial Frontage
-  City Boundary
-  Burquitlam – Lougheed Neighbourhood Plan Boundary

Notes:

- All other streets will conform to the City of Coquitlam’s *Subdivision and Development Servicing Bylaw No. 3558,2003*.
- For frontages along North Road, these guidelines pertain to the lands within Coquitlam city limits only.





2.0 STREETScape DESIGN PRINCIPLES

2.1 STREETScape HIERARCHY LEVELS

The streetscape hierarchy is comprised of ‘levels’, as illustrated in **Figure 3**.

Streets that are not identified as being **Levels 1-3** in **Figure 3** will defer to the standard established in the city of Coquitlam *Subdivision and Development Servicing Bylaw No. 3558, 2003*.

Specifications on the applicable paving materials lighting, landscaping and site furnishings identified in each ‘Level’ are provided in section **4.0 Site Furnishings**.

2.2 ALL STREETScape LEVELS

- a) Provide enhanced streetscape infrastructure for banners and seasonal lighting receptacles on the street light poles.
- b) Patio and outdoor display areas are encouraged for all **Commercial Frontage** types. In addition, storefronts and café fronts with ‘garage door’ style openings can help achieve vibrancy.
- c) Uses in the **Building Frontage Zone** could include public seating, outdoor eating associated with restaurants and coffee shops and outside display of merchandise provided that the dimensions of other **Zones** are maintained and the objectives of the City of Coquitlam’s *Encroachment Policy* are upheld.
- d) The **Building Frontage Zone** may be used as a secondary siting area for elements of the streetscape that are usually in the **Furnishings Zone**. Where feasible, the minimum private property offset will be increased to allow commercial uses to “spill” onto the sidewalk without impeding the movement zone.
- e) Ventilation structures, controller and other utility vaults, boxes and kiosks should be located underground, their tops flush with the paved surface. Covers, hatches and grates should have a surface texture that is slip-resistant.
- f) Include concrete bands or expansion joints along property lines in hard surface areas.
- g) Align jointing patterns with the outside edges of the planters and the seating pockets.



2.3 STREETScape LEVELS 1A & 1B

Streetscape Levels 1a and **1b** apply to commercial-fronting portions of North Road and Clarke Road, as indicated in **Figure 3**, where the highest density of mixed-use development occurs. These streetscapes are urban in character and accommodate a strong pedestrian presence.

- › Pedestrian Crosswalks will be wider than the Citywide standard to accommodate higher numbers of users around Evergreen Line stations.
- › The **Pedestrian Movement Zone** will be at least 3.0 metres wide.
- › The **Furnishing Zone** will be at least 2.5 metres wide.



2.4 STREETScape LEVELS 2A & 2B

Streetscape Levels 2a and **2b** apply to portions of Lougheed Highway, Austin Avenue, Como Lake Avenue and North Road located on the perimeter of areas with High Density multiple-family development. They are urban in character and accommodate a strong pedestrian presence. In addition, this streetscape has a 'supportive' functional significance in the public realm.

- › Pedestrian Crosswalks will be wider than the Citywide standard where sites allow to accommodate higher numbers of users around Evergreen Line stations. These sidewalks will narrow to the citywide standard at sidewalk let-downs and crosswalks .
- › The **Pedestrian Movement Zone** will be at least 2.0 metres wide.
- › The **Furnishing Zone** will be at least 2.5 metres wide.



2.5 STREETScape LEVEL 3

Streetscape Level 3 applies to heavy traffic commercial areas that radiate outwards from the dense centre and are typically adjacent to residential high and medium density development. They are urban in character and accommodate a moderate pedestrian presence, where commercial use represents the best use of at-grade space.

Figure 4: Streetscape Level 1a: Commercial Frontage

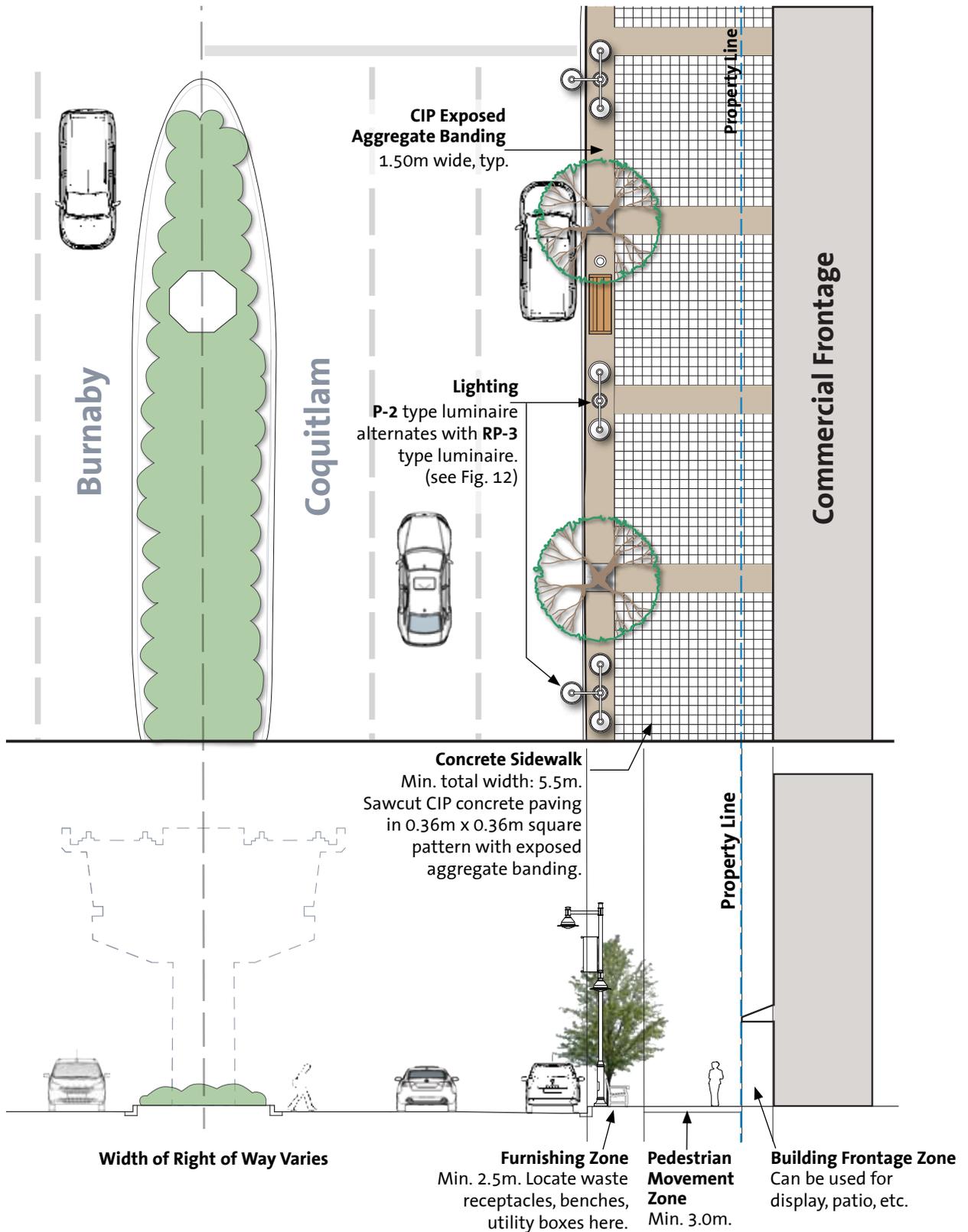


Figure 5: Streetscape Level 1b: Commercial Frontage

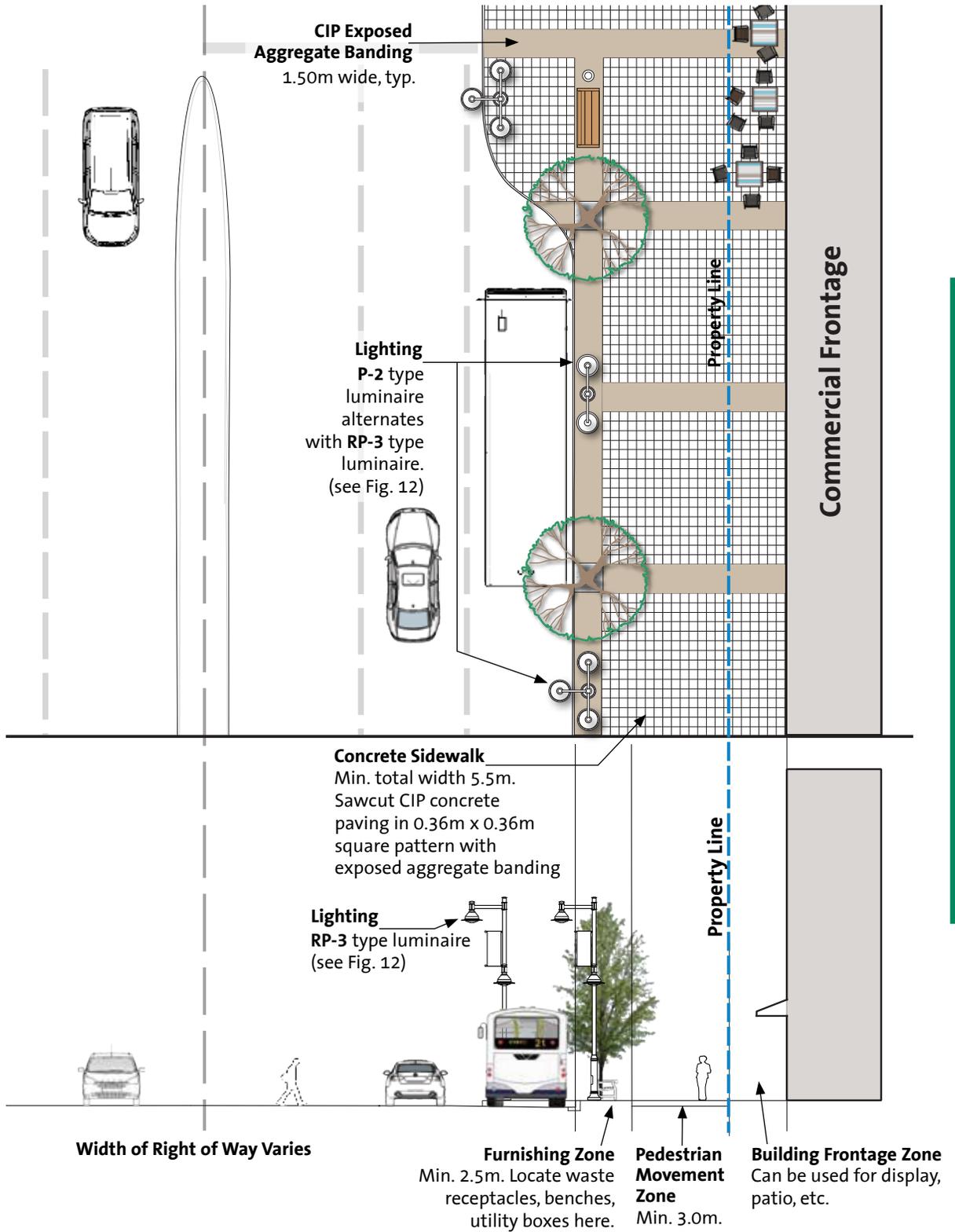


Figure 6: Streetscape Level 2a: Residential Frontage

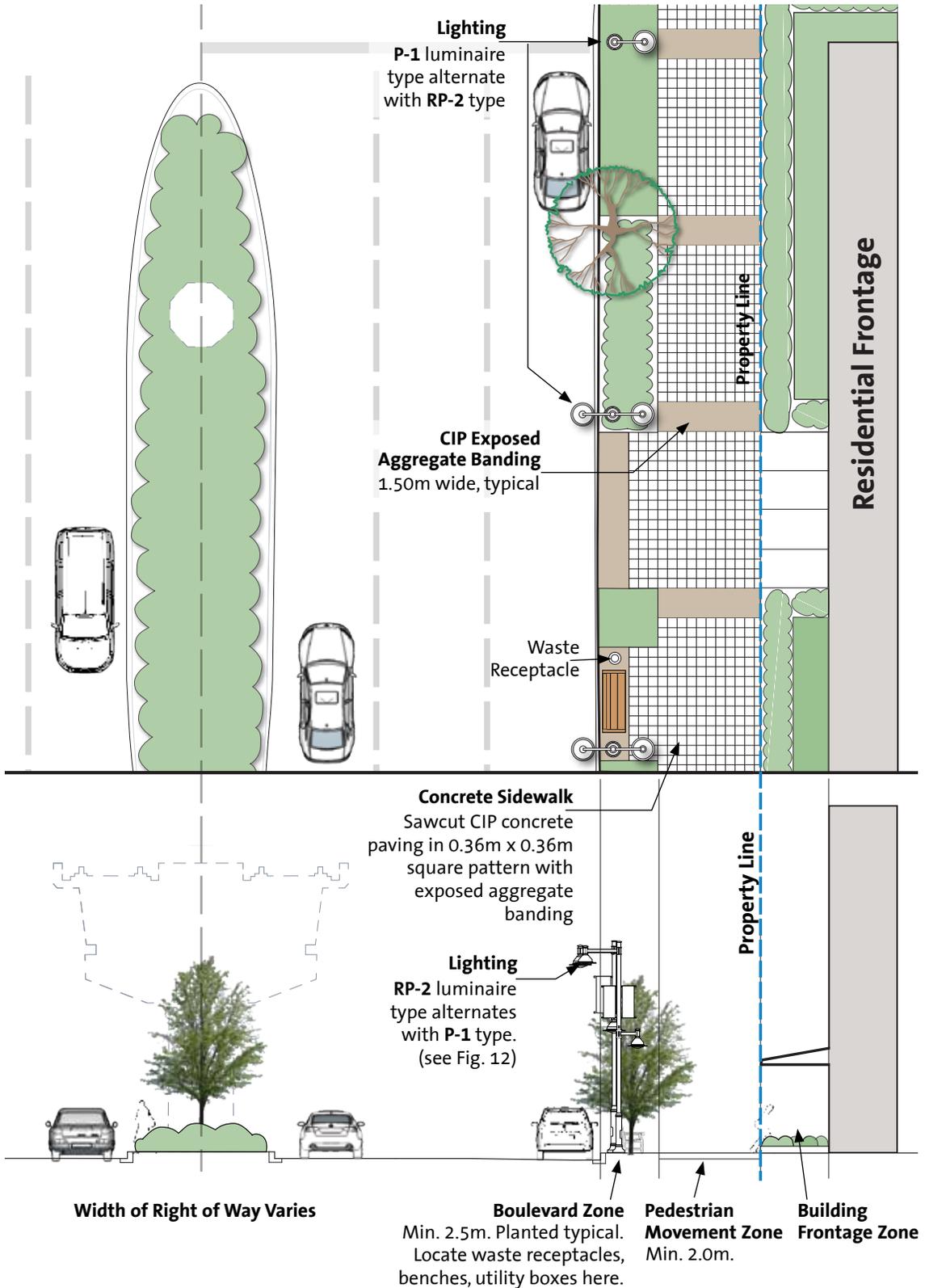


Figure 7: Streetscape Level 2b: Residential Frontage

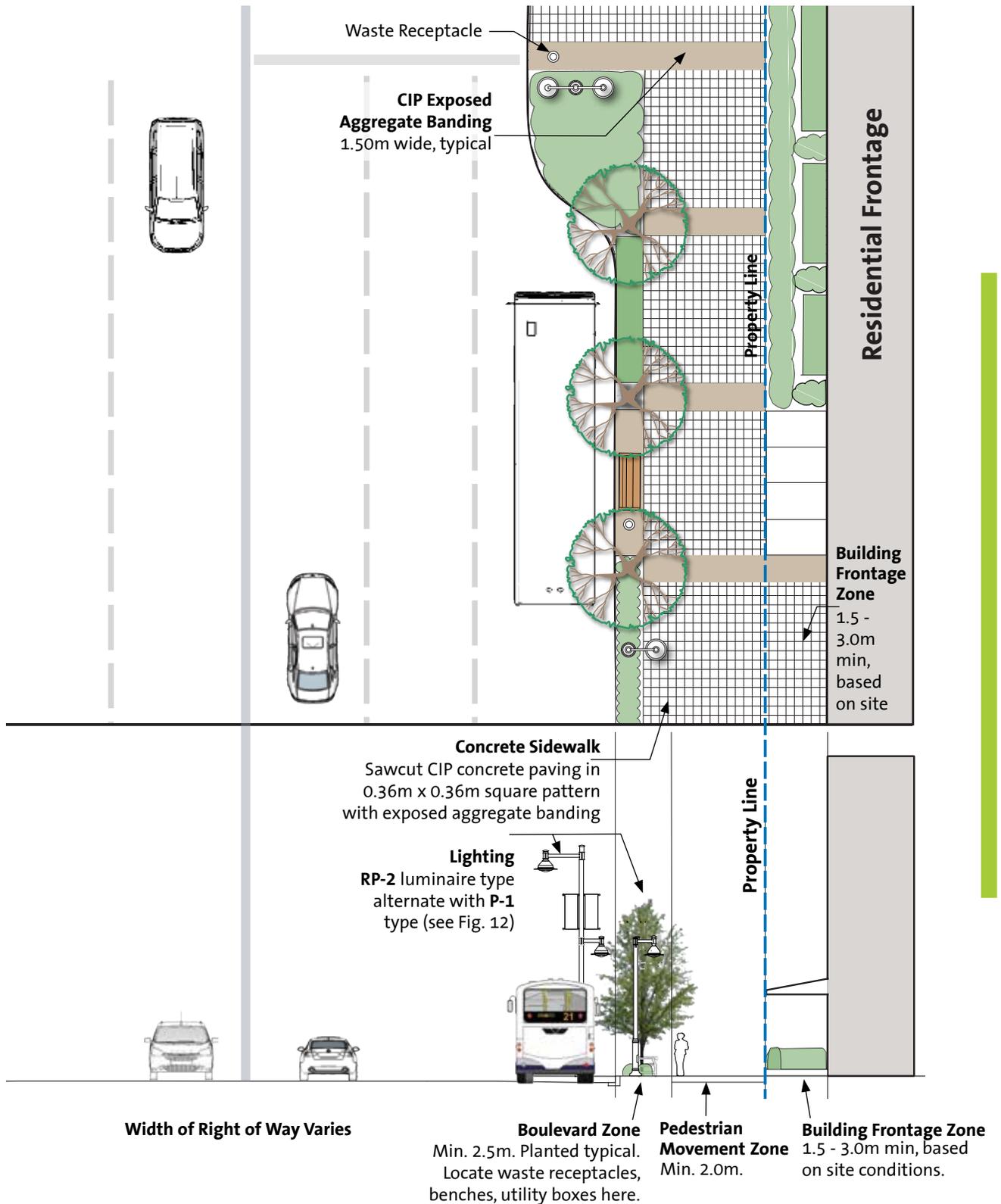




Figure 8: Streetscape Level 3: Commercial Frontage

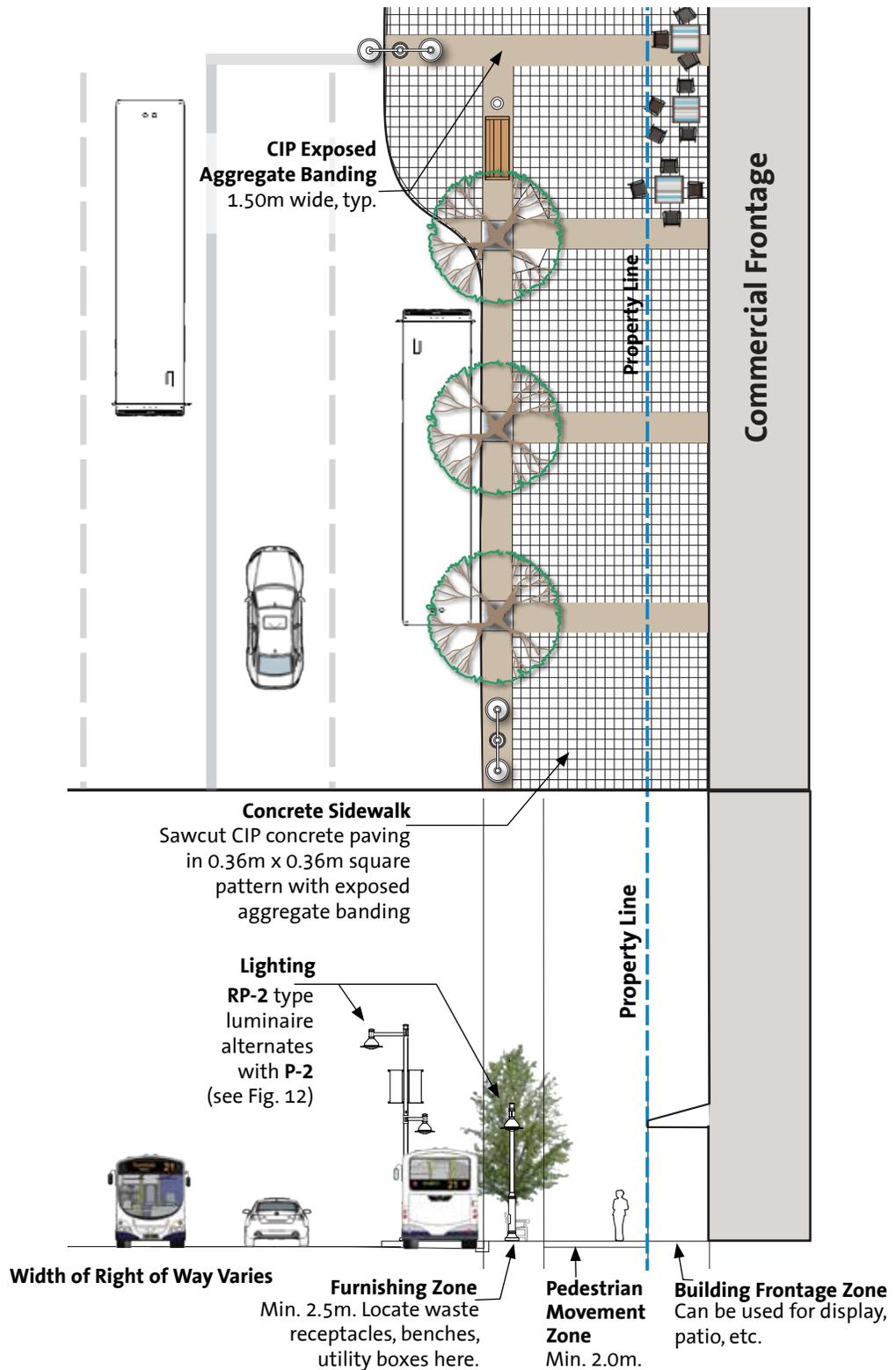


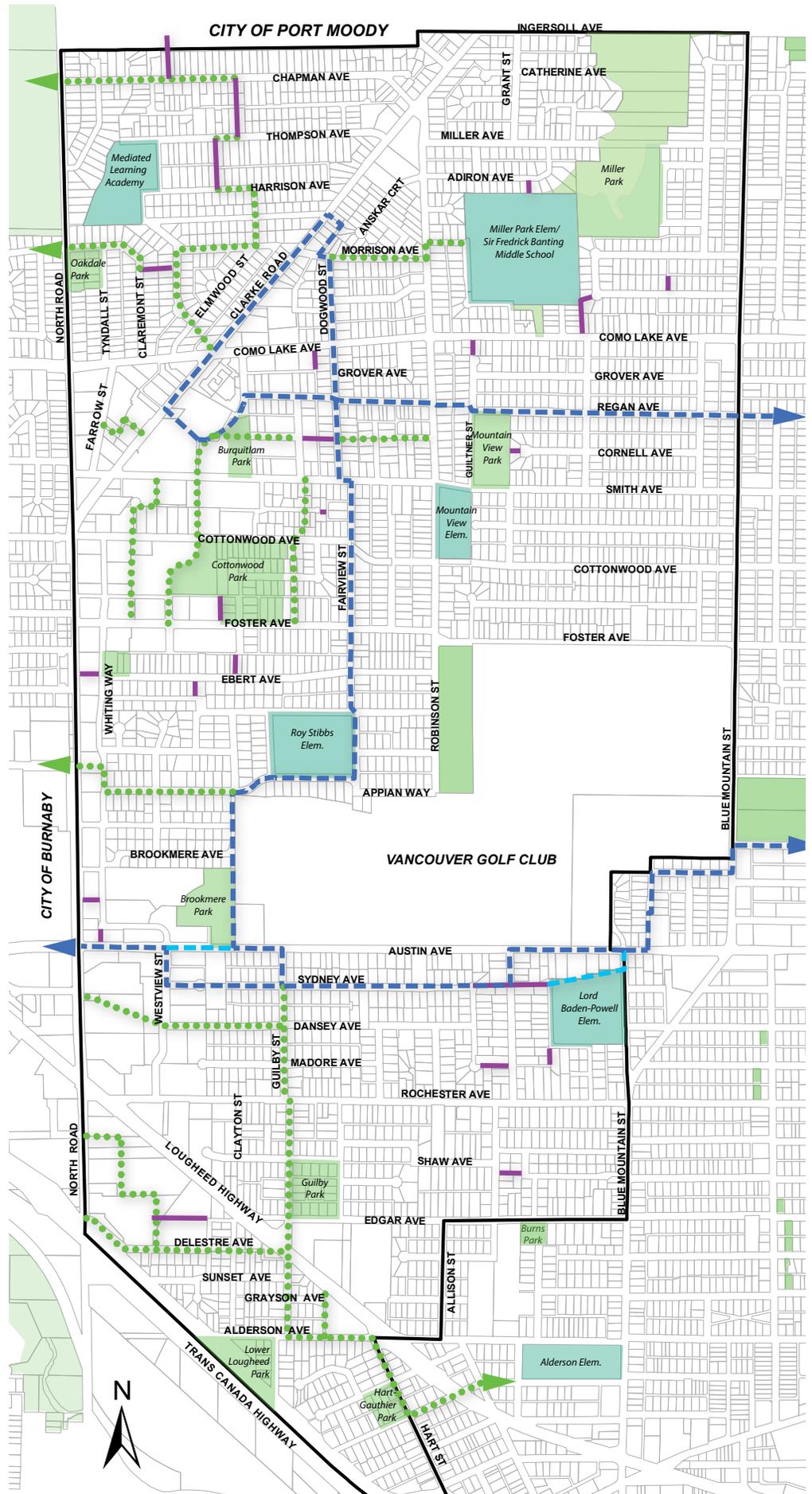
Figure 9: Greenway Network

Legend

-  Proposed Citywide Greenway
-  Proposed Citywide Greenway Alternate
-  Proposed Neighbourhood Greenway
-  Existing Walkway
-  Parks
-  Schools
-  Burquitlam – Lougheed Neighbourhood Plan Boundary

Notes:

- Citywide Greenways will be built by the city.
- Neighbourhood Greenways will be built together with adjacent redevelopment.
- Citywide Greenways are identified in Coquitlam’s *Strategic Transportation Plan* and are intended to be continuous routes that link major destinations throughout the city, including major commercial centres, schools, parks, amenities, and SkyTrain stations.
- Three major citywide Greenways have been identified:
 - **Dogwood-Fairview Greenway**
 - **Smith-Regan Greenway**
 - **Austin-Sydney Greenway**

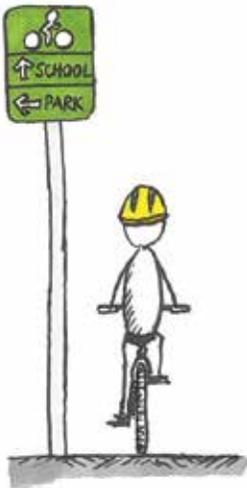




3.0 GREENWAY AND NODE DESIGN

3.1 GREENWAYS

Coquitlam's *Strategic Transportation Plan* (2012) includes a comprehensive network of **Citywide Greenways** which will be funded by the City as capital projects from Development Cost Charges (DCCs). This is supported and further connected by a network of **Neighbourhood Greenways** in the BLPN area which will be funded as off-site improvements during redevelopment. Greenways are intended to be multi-modal corridors that encourage and support walking and cycling for both recreational and commuting purposes.



These guidelines establish a distinct aesthetic and character to identify multi-modal routes which differ from the standard sidewalk. Furthermore, this visual difference helps to promote non-vehicle movement within Burquitlam – Lougheed and the City of Coquitlam. The unique character also helps to identify to cyclists and pedestrians that they have arrived in Burquitlam – Lougheed from adjacent portions of the City.

3.1.1 Citywide Greenways

Citywide Greenways are intended to be continuous routes that strategically link major destinations throughout the City, including major commercial centres, schools, parks, and other community facilities, as shown in **Figure 9**. The streetscape improvements that accompany **Citywide Greenways** are generally funded through Development Cost Charges.



Greenways can take a variety of forms, depending on the local context



3.1.2 Neighbourhood Greenways

Neighbourhood Greenways are generally shorter and provide connections within the City Centre and Neighbourhood Centres, as well as connections to the **Citywide Greenway** network and new and enhanced connector pathways, where opportunities exist, as an opportunity to enhance pedestrian connections and shorten walking distances. **Figure 9** shows the **Neighbourhood Greenways** in the Burquitlam – Lougheed Neighbourhood.

Neighbourhood Greenways are paid for by frontage improvements that accompany individual site redevelopment projects and are completed at that time.

The City supports the development of a network of **Citywide** and **Neighbourhood Greenways** made up of both on-street and off-street facilities throughout the community. Enhanced treatments distinguish these greenways from other cycling and walking routes. Potential treatments along **Citywide** and **Neighbourhood Greenways** are shown in **Table 1**.

The specific design treatments for individual greenways will be determined through functional design at the time of their development.

Table 1: Greenway Design Elements

Feature	Local Streets	Collector and Arterial Streets	Off-Street
Enhanced sidewalk width	✓	✓	
Local street bikeway (on low volume roadways)	✓		
Multi-use pathway - MUP (continuous, accessible path on one side of the street)		✓	✓
Additional boulevard planting	✓	✓	✓
Narrower crossings (curb extensions)	✓	✓	
Traffic calming (discourage speeding and short-cutting)	✓		
Street furniture (e.g., park benches and water fountains)		✓	✓
Pedestrian lighting		✓	✓
Public art (interpretive signage)	✓	✓	✓
Alternative stormwater management	✓		

Notes: Curb bulges on arterials will only be considered in conjunction with parking pockets, or where all mobility / capacity needs are met.

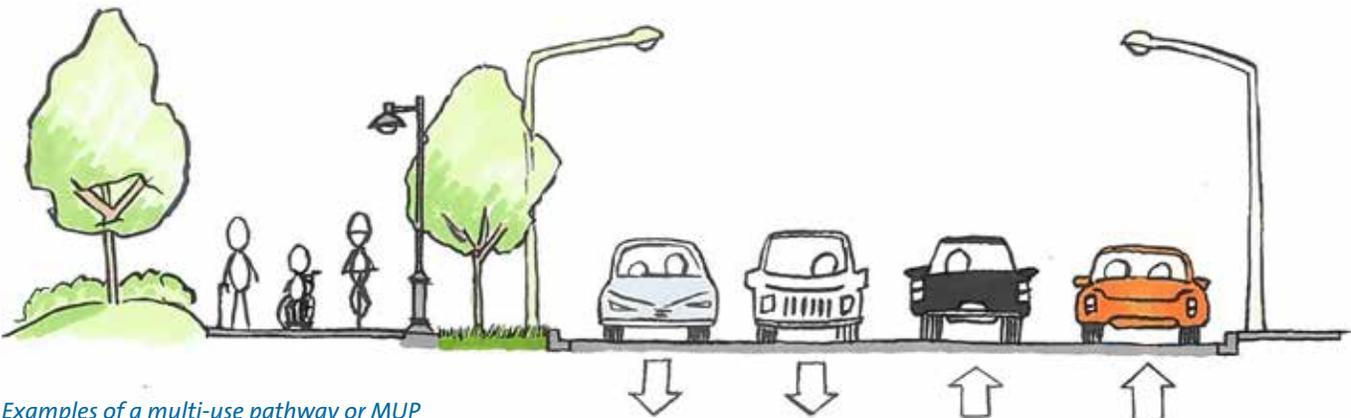


Design intent:

- To have majestic, long-lived and healthy street trees throughout the Burquitlam – Lougheed neighbourhood;
- To have long-lived and healthy large-scale street trees;
- To have street trees that address storm water management, biodiversity, and that offer great environmental benefits;
- To create a unique sense of place for Burquitlam – Lougheed within the greater city context;
- To provide pleasant places for walking and cycling.

Guidelines:

- Develop a clear visual cue to identify **Greenways** as a multi-modal route within the neighbourhood.
- Establish a uniform treatment of **Greenway** surfacing and layout to reinforce the use and character.
- Regularly spaced **nodes** provide rest areas at the top of hills to support a diversity of ages and abilities.
- Create **nodes** with layouts and key elements reflective of Burquitlam – Lougheed’s history.



Examples of a multi-use pathway or MUP

3.2 NODES

Nodes are strategic focal points for orientation like squares and junctions. As they are typically located along transportation routes, they are suitable locations for a public resting or gathering place. They are usually located at intersections along streets or **Greenways**. **Node** locations are identified on *Schedule E* of the *BLNP* and are shown in **Figure 10**.

Nodes form an integral part of the **Greenway** network by supporting neighbourhood and city-wide walkability. They provide a place to sit for seniors and children in a public plaza or green space. They represent an important informal place in the community.

Node dimensions:

- › **Primary Nodes** include a public gathering space of at least 250m².
- › **Major Nodes** will be defined by a plaza of at least 45m².
- › **Minor Nodes** will include a plaza or green space of at least 16m².

3.2.1 Primary Nodes - Requirements & Guidelines

Burquitlam Neighbourhood Centre and Lougheed Neighbourhood Centre are identified as **Primary Nodes**. They are unique and important commercial precincts for the Burquitlam – Lougheed area. North Road and Clarke Road will have increased residential densities, resulting in higher demands on the streetscape corridor (see **Figure 3**).

As illustrated in **Figure 10**, a **Citywide Greenway** bisects these Neighbourhood Centre areas. Design requirements identified for the two Neighbourhood Centres and the North Road / Clarke Road Corridor will need to be coordinated and integrated with the future implementation of the **Citywide Greenway**.

Primary Nodes provide a public open space that is a minimum of 250m² and shall include elements such as:

- › A portion of weather protection coverage for year-round use;
- › Seating and other appropriate street furniture including bicycle storage;
- › Public art of a scale that highlights the significance of the space;
- › A mix of hard and soft landscaping;
- › Pedestrian oriented lighting that can illuminate the space for evening programming;
- › Wayfinding;
- › Fronting buildings will have active frontages.

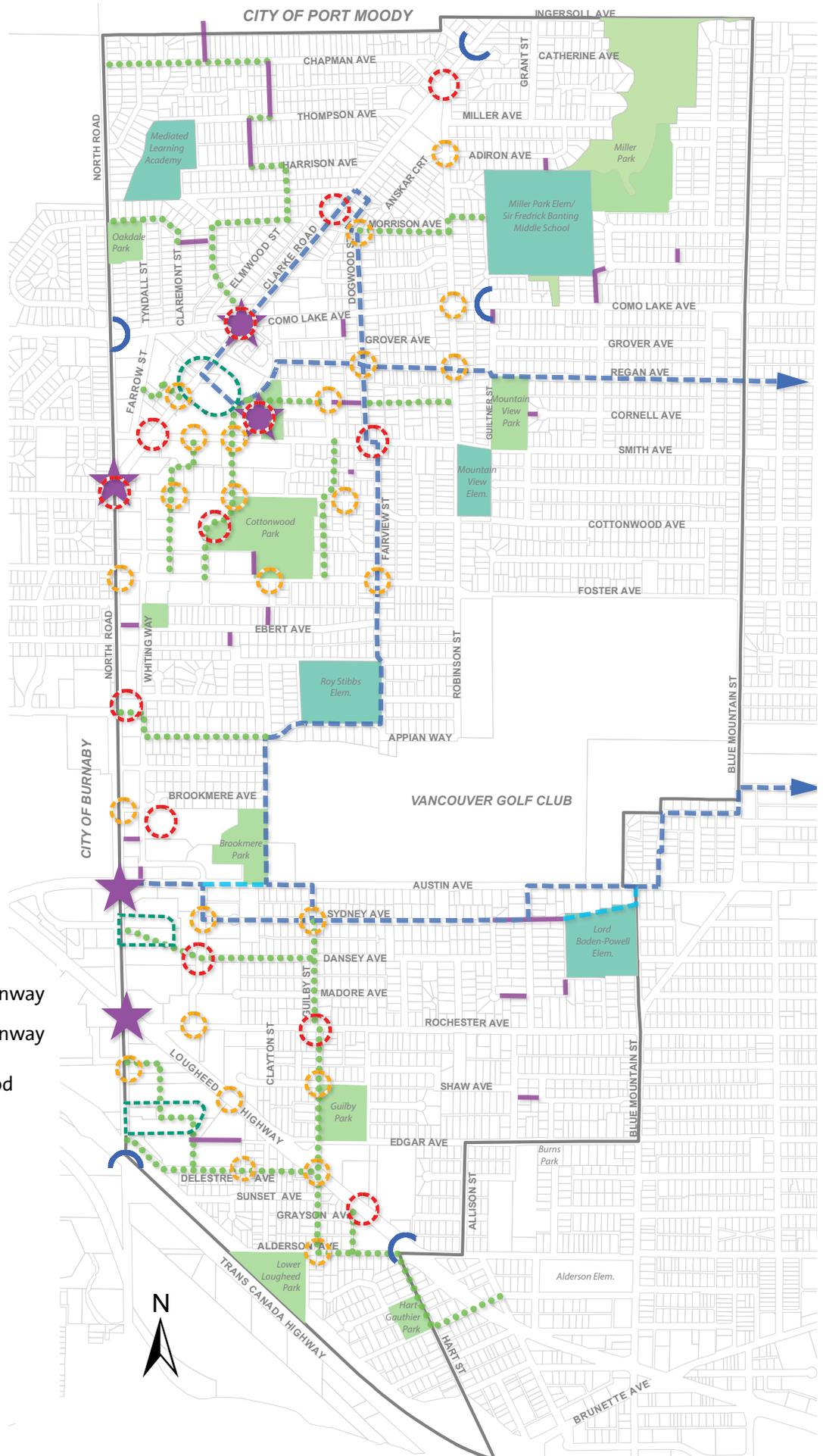
Guidelines:

- a) Ensure layout and key elements are reflective of Burquitlam and Lougheed history;
- b) Provide streetscape infrastructure for banners and seasonal lighting on the light poles;
- c) Locate infrastructure for banners on buildings or building frontages;
- d) Provide increased sidewalk width beyond the minimum standard and ensure smooth pedestrian routes through the node.

Figure 10: Landmark, Node and Gateway Locations

Legend

-  Primary Node
-  Major Node
-  Minor Node
-  Landmark
-  Gateway
-  Proposed Citywide Greenway
-  Proposed Citywide Greenway Alternate
-  Proposed Neighbourhood Greenway
-  Existing Walkway
-  Parks
-  Schools
-  Burquitlam – Lougheed Neighbourhood Plan Boundary



3.2.2 Major Nodes

Major Nodes, as identified in **Figure 10 / Schedule E** of the BLNP, will provide a public open space that is a minimum of 45m². **Major nodes** encourage public gathering and lingering and shall include elements such as:

- › Seating and other appropriate street furniture;
- › Public art;
- › A mix of hard and soft landscaping;
- › Pedestrian oriented lighting;
- › Wayfinding;
- › Fronting buildings shall address the node with active frontages, architecturally distinctive façades, and high-quality streetscape treatments; and,
- › Where node spaces are adjacent to commercial retail units (CRUs), cafés, coffee shops and ice cream parlours are encouraged.

Guidelines:

- a) Provide streetscape infrastructure for banners and seasonal lighting on the light poles per **Matrix 1**.
- b) Locate infrastructure for banners on buildings or building frontages.
- c) Provide increased sidewalk width beyond the minimum standard along exterior frontages where the width of the right of way can accommodate it.
- d) Plant boulevards according to the preferred boulevard plan and street tree lists and in coordination with City Urban Forestry and Parks Planning staff. Particular emphasis should be placed on integrating plants as a prominent character element on all frontages.

3.2.3 Minor Nodes

Development located adjacent to **Minor Nodes**, as identified on **Figure 10**, is to provide a public open space that is a minimum of 16 m². **Minor nodes** will be designed to encourage public gathering and lingering and shall include elements such as:

- › Seating and other appropriate street furniture;
- › Public art;
- › A mix of hard and soft landscaping; and
- › Fronting buildings shall address the **node** with active frontages, architecturally distinctive façades, and high-quality streetscape treatments.

Guidelines:

- a) Provide increased sidewalk width beyond the minimum standard where width of right of way can accommodate it.



3.3 GATEWAYS

Gateways, as identified in **Figure 10 / Schedule E** of the BLNP, are distinct public places that represent ideas or events that are important to the community.

Gateways are ideal locations for public art and memorials to historic people and events and enhanced public realm design.

Gateways should include elements such as:

- › Public art;
- › Signage;
- › Unique public spaces and plazas;
- › Distinctive lighting;
- › Distinctive building facades;
- › High quality site furnishing;
- › Improved street presence.

3.4 LANDMARK SITES & BUILDINGS

Landmarks are external points of orientation, usually an easily identifiable physical object in the urban landscape. They are ideal locations for public art.

Buildings located at **landmark** sites and/or over 30 storeys in height will have a greater impact on the neighbourhood aesthetic character and will be required to achieve a higher level of design excellence. They are required to have enhanced public realm design. A rigorous design review process for these sites and buildings is outlined in the BLNP, section 4.3.

Landmark sites are identified in **Figure 10 / Schedule E** of the BLNP, whereas landmark buildings are either buildings located at landmark sites or buildings greater than 30 storeys in height.

“In lively, safe, sustainable and healthy cities, the prerequisite for city life is good walking opportunities . . . a multitude of valuable social and recreational opportunities naturally emerge when you reinforce life on foot.”

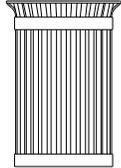
– Jan Gehl, *Cities for People*

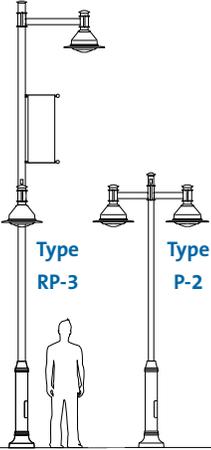
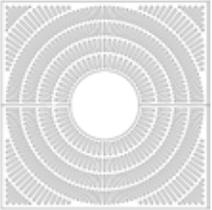


High quality public spaces include safe places to rest, supporting walkable neighbourhoods for residents of all ages

4.0 SITE FURNISHINGS

Matrix 1: Street Furnishing Palette

Feature	Specifications	Streetscape		
		Level 1a & 1b	Level 2a & 2b	Level 3
<p>Benches</p>  <p>Level 1 - benches include medallion Level 2 and 3 - no medallion</p>	<p>Metal framed bench with wood slat seat and back;</p> <p>Supplier: Frances Andrew Type: G24-311 Model: Garden Series Bench Colour: Black with Ipe Hardwood slat Material: Standard Aluminum Cast Finish: Polyester Powder Coating Supplier: Frances Andrews Site Furnishings Ltd, Surrey, BC Sustainability: Recycled content</p>	<p>With medallion Minimum four per block.</p>	<p>No medallion Minimum four per block.</p>	<p>No medallion Two located at major node.</p>
<p>Garbage Receptacles</p> 	<p>Steel receptacle with vertical slats;</p> <p>Supplier: Frances Andrew 'Carriage Lane' Series Type: R-31 Colour: Black Installation: per manufacturer's specifications or approved equivalent;</p>	<p>minimum two per block</p>	<p>minimum two per block</p>	<p>minimum one per block</p>
<p>Bollards</p> 	<p>Powder coated carbon steel;</p> <p>Supplier: Reliance Foundry Type: R-7901 Height: 36in Weight: 38lbs Colour: Black Installation: various available - concrete embedment, bolt-down flange, removable receivers and fold-down mountings. Accessories: Variety of chain links and padlock</p>	<p>where required</p>	<p>where required</p>	<p>where required</p>
<p>Bike Racks</p> 	<p>Supplier: Landform Model: Bola Colour: Stainless Steel Material: Stainless Steel Finish: Powder coat Sustainability: Recycled content, Recyclable Installation: embedded Leed 2009: SS Credit 4.2 Alternative Transportation—Bicycle Storage and Changing Rooms / MR Credit 4, Recycled Content / MR Credit 5, Regional Materials</p>	<p>at least two per block</p>	<p>at least one per block</p>	<p>at least one per block</p>
<p>Fencing</p>	<p>Fencing required to control pedestrian movement should be painted metal in rectilinear forms with vertical dominating over horizontal (non-climbable).</p>	<p>where required</p>	<p>where required</p>	<p>where required</p>
<p>Drinking Fountains</p>	<p>Optional, sturdy and vandal resistant</p>	<p>where required</p>	<p>where required</p>	<p>where required</p>

Feature	Specifications	Streetscape Level							
		1a	1b	2a	2b	3			
Roadway and Pedestrian Lighting 	Philips Lumec Transit Series configurations as illustrated in Figure 12 . To be coordinated with pedestrian lighting. Include banner arm pairs for commercial frontage streetscapes. Luminaire: TR20 Lamp Type: LED Mounting: SN Finish: Powder coat Colour: Black Base: SM	Type RP-3 alternate with type P-2	Type RP-2 alternate with type P-1	Type RP-2 alternate with type P-2					
		Include Banner Arms and Duplex Receptacles on pole?					✓	X	✓
Tree Grate 	Supplier: Dobney Foundry, Surrey, BC Type: ST48 Colour: Black Material: Ductile Iron Finish: Unfinished or Oiled Weight: 240 lbs approximate Sustainability: Recycled content, recyclable Installation: Concrete surround with 48" tree grate frame	✓	X	✓	X	✓			

Notes:

The locations and frequency of site furnishing elements are to be determined during functional design on a case - by - case basis at the time of redevelopment. As a rule, the quantity and location of these furnishings should be generously applied, as reasonable and appropriate, and in concurrence with other perimeter works and services and as determined by the General Manager Planning and Development.

Developers may propose additional streetscape design components beyond the minimum provisions identified for each frontage type, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager Planning and Development.

4.2 Paving Materials

All paving in publicly accessible areas should be chosen with regard for maximum accessibility and safety for those with mobility impairments, wheelchairs, canes, walkers, strollers and bicycles.

Materials

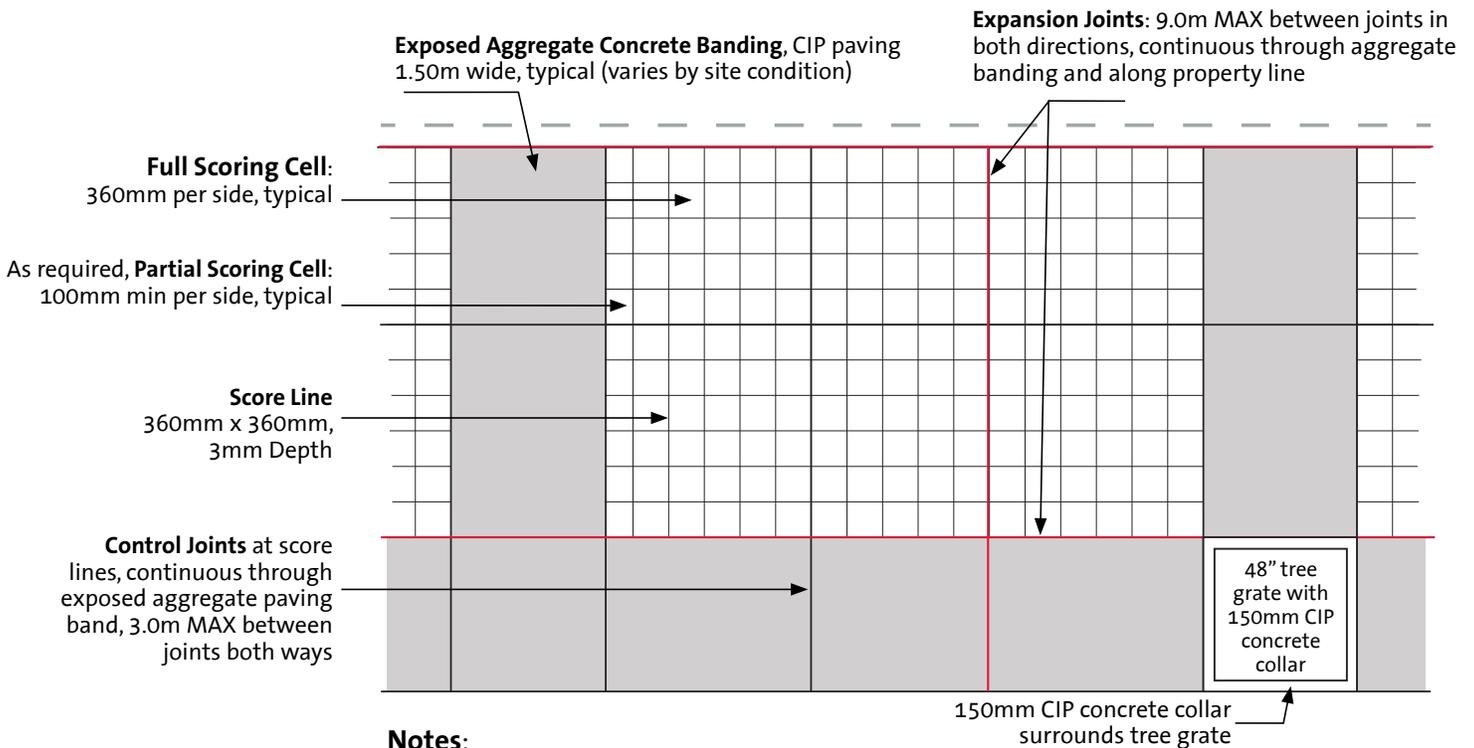
Simple paving finishes of scored natural and coloured concrete are used within the Neighbourhood Centres to allow a strong visual context for Burquitlam – Lougheed as illustrated on **Figure 11**.

The **Pedestrian Movement Zone** has a simple surface with subtle textural changes helping to identify seating areas in **Nodes**.

Figure 11: Concrete Paving Pattern

Plan

Drawing not to scale



Notes:

1. Joint pattern may vary slightly with reduced widths of banding.
2. "Scoreline" is a 3mm x 3mm dimension joint. They are to be saw-cut. Trowelling will not be accepted.
3. Tree grate - Dobney Foundry ST48 - see **Matrix 1**.



4.3 Lighting

Lighting elements will reflect the features of the existing streetscape and build upon that aesthetic with currently available, energy efficient and durable poles and current technology luminaire options. The lighting products listed in this section will be applied to the **Streetscapes Levels 1, 2, and 3** as shown in **Figures 4 - 8**.

Recommended hardware for **Streetscape Levels 1, 2 and 3** is:

- › Philips Lumec Transit series TR-20 luminaires and poles with powder coat finish in semi-gloss textured black.

Poles located on all commercial street frontages as shown in **Figure 3** will:

- › accommodate banners as specified by the City of Coquitlam. Poles in these areas will be equipped with banner accessories.
- › come equipped with exterior rated GFCI plugs to enable seasonal lighting installations. A guy wire system at a minimum elevation of 2.4m should be provided between poles without the plugs to support low voltage lighting extensions.
- › Roadway lighting should be paired with pedestrian lighting wherever possible.

Figure 12 shows preferred configurations of luminaires and poles. Developers may propose additional streetscape design components beyond the minimum provisions identified for each frontage type, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager Planning and Development.

4.3.2 Roadway Lighting

Guidelines:

Some example configurations are provided in **Figure 12** but developers should work out the correct spacing, arm lengths and fixture height based on industry standard modeling and requirements, BC Building Code, road classification, and City of Coquitlam requirements. The spacing of the light standards will be optimized to reduce impacts on street trees.

An example of Lumec Transit series Roadway / Pedestrian configuration type RP-3 with banner hardware



4.3.3 Pedestrian Lighting

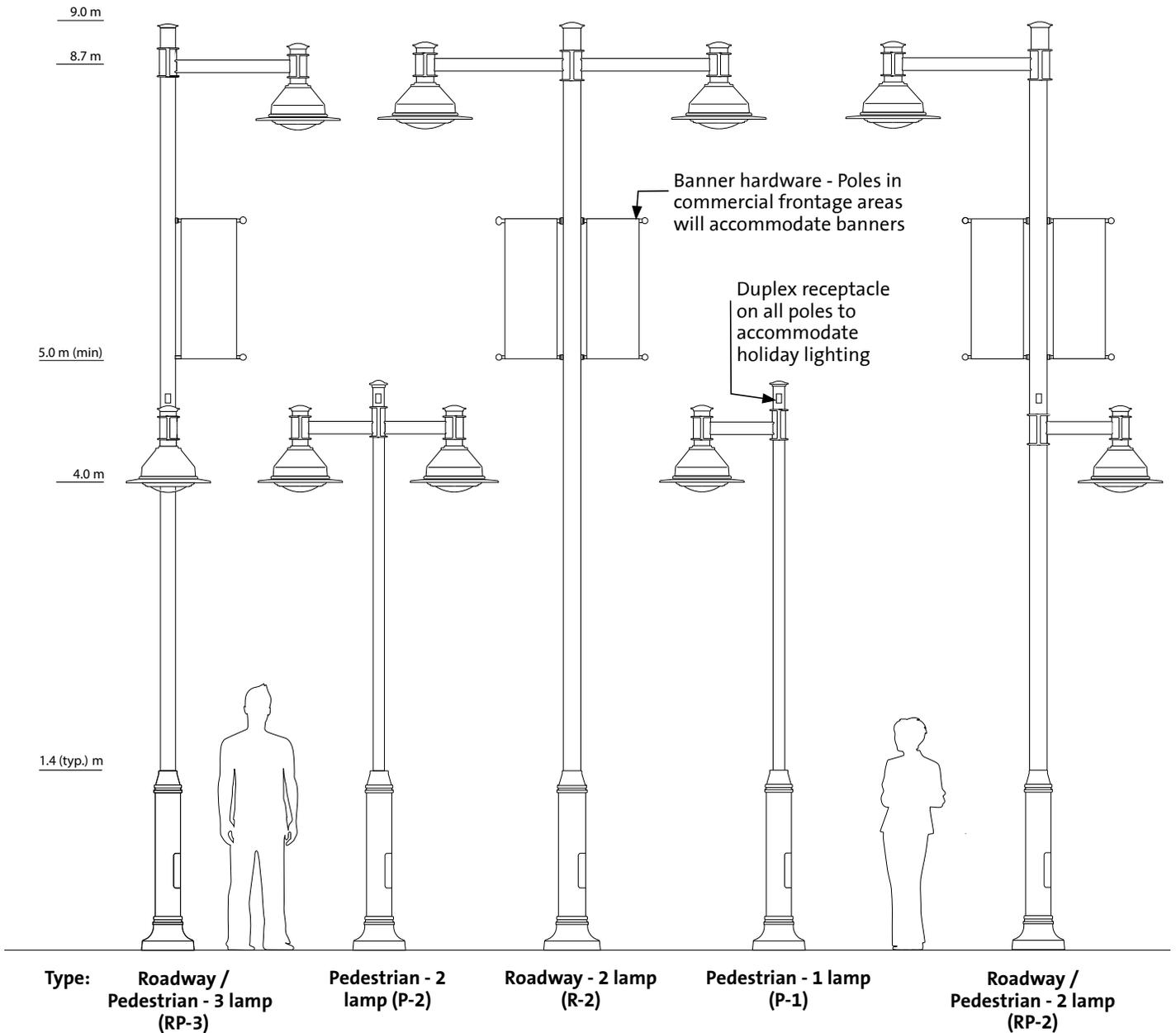
Guidelines:

Some example configurations are provided in **Figure 12**. Developers will optimize the placement, model, and spacing within the current requirements to ensure uniformity, safety, and universal accessibility is achieved for all users.

4.3.4 Node and Plaza Lighting

Safe and comfortable lighting will be provided for **Primary, Major and Minor Nodes**. Lighting levels are to meet with industry standards, BC Building Code and the City of Coquitlam requirements. Pedestrian level lighting rather than roadway level lighting fixtures should be used. An exterior rated GFCI plug to enable seasonal lighting installations should be provided at a minimum elevation of 2.4m.

Figure 12: Example configurations of approved Roadway & Pedestrian lighting options



Manufacturer: Philips Lumec
 Transit series
Luminaire: TR-20
Mount: SN
Colour: Semi-gloss textured black



4.3.5 Lighting on Non-Designated Streets

Decorative lighting is currently applied to high and medium density streets not designated by streetscape levels. Streets within the BLNP area not designated with a streetscape level will apply standards set in the City of Coquitlam's *Subdivision and Development Servicing Bylaw No. 3558, 2003*.

4.4 LANDSCAPING

Landscaping shall follow the city's "Approved Street Tree List" and shall comply with guidelines established by the City of Coquitlam's Urban Forestry.

Greenway and Node Plantings

Plantings in **nodes** should enhance the sense of place within Burquitlam – Lougheed as **gateways** to the neighbourhood, particularly for pedestrians and cyclists traveling along **Citywide Greenways**. A limited palette of plants with high aesthetic value and easy identification that are present throughout the neighbourhood should be used.

Street Trees

The appropriate street tree selection is critical to developing the character of the area and selecting species that are long lived and have minimal impact on the surrounding public realm elements allows for reduced maintenance costs. Street tree plantings should seek to establish a continuous tree canopy along primary street corridors of character areas. Appropriate tree selection will be guided by the *Tree Management Bylaw No. 4091, 2010* in conjunction with the City of Coquitlam's Urban Forestry Technician. Tree and underground utility placement must be coordinated to adequately accommodate both and avoid conflict.

Building Frontage Zone

Plantings in commercial frontage areas should, if there are no plantings on the development parcel, flow into the **Building Frontage Zone** as an extension of the **Furnishing** or **Boulevard Zone** plantings to create a strong sense of continuous public realm from curb to building face. In residential frontage areas, or areas of lower density residential, development parcel plantings should come up to the back of the sidewalk where there are no utility easement restrictions. Planting will be guided by the City of Coquitlam's preferred tree and planting lists.

Boulevard / Furnishing Zone Plantings

Select appropriate plantings for the **Furnishing Zone** and **Boulevard Zone** to add seasonal interest, biodiversity, colour, green character and environmental benefit. They will also help create a sense of place. The plans suggest critical areas where plantings should occur but also indicate that there are opportunities for developers and landowners to "adopt" portions of the **Boulevard Zone** for additional beautification with planting areas.

North Road was constructed in the mid nineteenth century by the British Royal Engineers to connect New Westminster to the year-round port facilities in Port Moody.

4.5 PUBLIC ART

Public Art is encouraged throughout the BLNP and is required at **Gateways** and some **Nodes**.

For general guidance on Public Art, please refer to Part 4 of the *Citywide OCP: Urban Design + Development Permit Areas* and the city of Coquitlam's *Public Art Policy and Project Plan* (2011).

In all cases, public art that is not incorporated into the pavement itself should be located outside the **Movement Zone** of all sidewalks and be either within the **Furnishings Zone**, where space is adequate such as at sidewalk bulges, or within the **Building Frontage Zone**, where it will not conflict with pedestrian movement to building entries or other desire lines.

Public safety is a universal criteria for all types of public art; art should not have sharp, protruding, or overhanging edges that can trip or catch a passer-by. In addition, features and associated lighting will be reviewed to ensure the features and associated lighting are not a distraction to road and sidewalk users.

Public art will be reviewed by municipal staff, with the advice of professional artists from the community to ensure that the art meets the criteria in the guidelines and is of artistic excellence and high quality.

Gateways, plazas, and **nodes** help commemorate a special place or event. Along with parks and greenways, they are excellent locations for public art, and gathering places in their own right. Please refer to *BLNP Schedule F: Districts* and *Schedule E: Urban Design Framework* to identify these locations.



Example Gateway Feature South End Median (south of Gatineau). The concept shows an elevated star structure in the road median. It features changeable accent lighting.



Conceptual Examples of Public Art as proposed for North Road

Conceptual artwork for bench medallions to be used in Streetscape Levels 1a & 1b, which recognize the significance of North Road and Clarke Road



4.6 WAYFINDING

Wayfinding systems connect residents and users to the City’s major facilities and projects, and allow individuals to easily enter a neighbourhood and easily navigate through it.

All **Primary** and **Major nodes** shall be provided with wayfinding as per the *Citywide Wayfinding Strategy*.

In addition, the City of Coquitlam has developed corridor themes for the Burquitlam-Lougheed Neighbourhood, which focus heavily on the strong history of this neighbourhood. Given North Road’s prominent place in history as the second oldest road in the lower mainland, running due North and connecting New Westminster with Port Moody, the North Road theme is heavily tied to the North Star, compass imagery and the concept of multimodal transportation.

Figure 13 shows examples of the City of Coquitlam’s selection of potential North-Road and Burquitlam-Lougheed Wayfinding Tools and Signage for reference.

Figure 13: Examples of Wayfinding Signage & Street Markers



5.0 IMPLEMENTATION

Over the next 20 to 25 years the new Burquitlam-Lougheed Neighbourhood Plan will guide growth and reinvestment in the neighbourhood to help create a complete community. This companion document is intended to ensure that as that growth occurs, and as the streetscapes in this neighbourhood evolve, they will be continually improved and will be guided by the core design objectives within this document.



Some examples of existing Node and Plaza spaces



A Residential streetscape showing a planted boulevard

