

October 30, 2017

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To: City Manager

From: General Manager Planning and Development

Subject: **Burquitlam-Lougheed Streetscape Guidelines**

For: **Council**

**Recommendation:**

That Council endorse the Burquitlam-Lougheed Streetscape Guidelines as outlined in the report from the General Manager Planning and Development, dated October 30, 2017 and entitled "Burquitlam-Lougheed Streetscape Guidelines".

**Report Purpose:**

This report presents the updated *Burquitlam-Lougheed Streetscape Guidelines* (Attachment 1), a companion document to the Burquitlam-Lougheed Neighbourhood Plan (BLNP) which is intended to update the previous streetscape guidelines while incorporating more recent design elements that have been introduced through Council approved or endorsed policy.

**Strategic Goal:**

The updated Burquitlam-Lougheed Streetscape Guidelines support the strategic goal of 'Strengthening Neighbourhoods'.

**Background:**

The existing Burquitlam-Lougheed Design Guidelines were endorsed by Council in 2008 and served as a companion document to the 2002 Burquitlam and Lougheed Neighbourhood Plans. The Design Guidelines established standards that have helped to create a strong neighbourhood character as redevelopment takes place throughout the Burquitlam-Lougheed area.

Since the endorsement of the 2008 Design Guidelines, Council has approved additional strategies and policies to improve design elements in the City. These include the *Strategic Transportation Plan (2012)*, the consolidated *Subdivision and Development Servicing Bylaw (2012)*, the *Transit-Oriented Development Strategy (2012)* and the new *Burquitlam-Lougheed Neighbourhood Plan (BLNP, 2017)*.

Council also supported the North Road and Clarke Road Corridor Enhancement Project on February 16, 2015 (Attachment 2) which established modern streetscape standards to areas near new Skytrain stations where redevelopment and street improvements would precede the new BLNP. The updated BLNP Streetscape Guidelines incorporate Council's previously approved design directions and build on recently completed capital projects, to provide a clear set of standards for future development in the BLNP area.

9.



**Discussion/Analysis:**

This update to the *Burquitlam–Lougheed Streetscape Guidelines* has been prepared to guide improvements to the overall look and feel of key public areas in the Burquitlam-Lougheed area.

These improvements include on-street parking areas, improved sidewalks, street furniture, public walkways and plazas, public areas, art, street trees and landscaping that will be built incrementally as redevelopment takes place.

The updated guidelines do not seek to introduce new design direction or elements, instead the proposed Guidelines are intended to update the 2008 Guidelines with current best practices while incorporating the capital improvements made during the North Road and Clarke Road Enhancement Project and turning engineering drawings into guidelines for future development.

***Streetscape Levels***

The areas where enhanced streetscapes are to be constructed, originally established in the 2008 Design Guidelines, have been carried over in the new Streetscape Guidelines and reflect the revised land-uses, allowable densities, traffic pattern changes, street frontage requirements and transit-oriented development in the BLNP.

Building on the approved and constructed Clarke Road and North Road Corridor Enhancement Project, five levels of streetscape are outlined in these guidelines.

The new streetscape levels (see Attachment 1, Figure 3) include Levels 1a and 1b along portions of North Road and Clarke Road, where development density will be highest, frontage is commercial use, and developments will be mixed-use. The distinctive differences set out in the Clarke Road and North Road Corridor Enhancement project necessitate separate requirements for the two distinctive portions of the corridor.

Levels 2a and 2b apply to portions of Lougheed Highway, Austin Avenue, Como Lake Avenue and North Road along the perimeter of high-density development and where the urban character supports a strong pedestrian presence.

Level 3 streets are located along heavy-traffic areas that radiate outwards from the core where urban character is moderate and commercial uses are located at-grade. All other streets will continue to apply the standard requirements included in the City's *Subdivision and Development Servicing Bylaw* (2012).

**Discussion/Analysis: cont'd/*****Greenway and Node Design***

The Streetscape Guidelines have also been updated to reflect the vision of the BLNP and the Strategic Transportation Plan. The major Citywide Greenways are planned to be funded by the City as capital projects from DCC charges. These will be further connected by a network of Neighbourhood Greenways which typically are funded as off-site improvements during redevelopment (though other potential sources of partial funding for the Neighbourhood Greenways is currently being explored, subject to certain qualifying criteria e.g. applicable to smaller scale, low density infill redevelopment proposals). Both types of Greenways are intended to serve as multi-modal corridors encouraging and supporting walking and cycling for both recreational and commuting purposes.

The Streetscape Guidelines also clearly articulate the required dimensions, character, furnishing and development requirements for Nodes throughout the BLNP area to be developed as small plazas or parkettes. Node locations were established in the BLNP, and these spaces form an integral part of the Greenway network, supporting neighbourhood and city-wide walkability and providing a place to sit for seniors and children in a public plaza or green space.

***Site Furnishing***

Site furnishing requirements from the 2008 Design Guidelines have been updated in the Streetscape Guidelines to meet specifications that have been determined in more recent plans, such as the Clarke Road and North Road Enhancement Project and Burquitlam Station Plaza Design.

***Wayfinding and Identity***

The Streetscape Guidelines have brought together elements of both the *Citywide Wayfinding Strategy* as well as corridor themes previously identified for the Burquitlam-Lougheed area through the North Road and Clarke Road Enhancement Project. These updated wayfinding measures connect residents and users to the City's major facilities and projects, and allow individuals to easily identify the neighborhood when they enter it.

The already-constructed streetscapes along the North Road and Clarke Road Corridor Enhancement Project focus themes specific to each road.

Given North Road's prominent place in history as the second oldest road in the lower mainland, running due north and connecting New Westminster with Port Moody, the previously supported North Road theme is heavily tied to the North Star, compass and multi-modal transportation.

Clarke Road's theme celebrates the evolution of travel over time. Originally built to provide a travel route with a more manageable grade, Clarke Road facilitated travel first by foot, then by horse, stagecoach, car and eventually multi-modal transportation today including Skytrain, bus and bicycle.

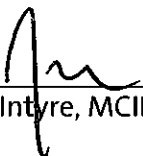
**Financial Implications:**

The BLNP and supporting documentation is identified as a key part of the Planning and Development Department's 2017 Work Plan and is being undertaken with existing staff resources, and is funded within existing budgets. Financial support for achieving Greenways in the community is as noted above, plus the option for partial funding of Neighbourhood Greenways is being explored through the Five-Year Financial Plan process.

**Conclusion:**

Over the next 20-to-25 years the new Burquitlam-Lougheed Neighbourhood Plan will guide growth and reinvestment in the neighborhood to help create a complete community. This companion document is intended to ensure that as growth occurs, and as the streetscapes in this neighborhood evolve, they will be continually improved and will be guided by the core design objectives within this document.

The updated BLNP Streetscape Guidelines are intended to be used as part of the design and review of public and private developments within the Burquitlam-Lougheed Neighbourhood Plan area. By updating the Guidelines to reflect policy and design direction that has been endorsed or approved by Council since 2008, the development community, staff and the public will have a greater degree of direction and certainty with respect to the form that future streetscapes will take in the BLNP area.



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J.L. McIntyre, MCIP, RPP

**Attachments:**

1. BLNP Streetscape Guidelines (Doc# 2740213)
2. Report to Council, dated February 8, 2015 "Evergreen Line Corridor – City Enhancement Plan Update (Doc# 1869899)

This report was prepared by Leifka Vissers, Community Planner with input from Transportation Planning, Development Servicing, Parks Planning and Design and Traffic Operations, and reviewed by Andrew Merrill, Manager Community Planning and Michelle Hunt, General Manager Finance and Technology.

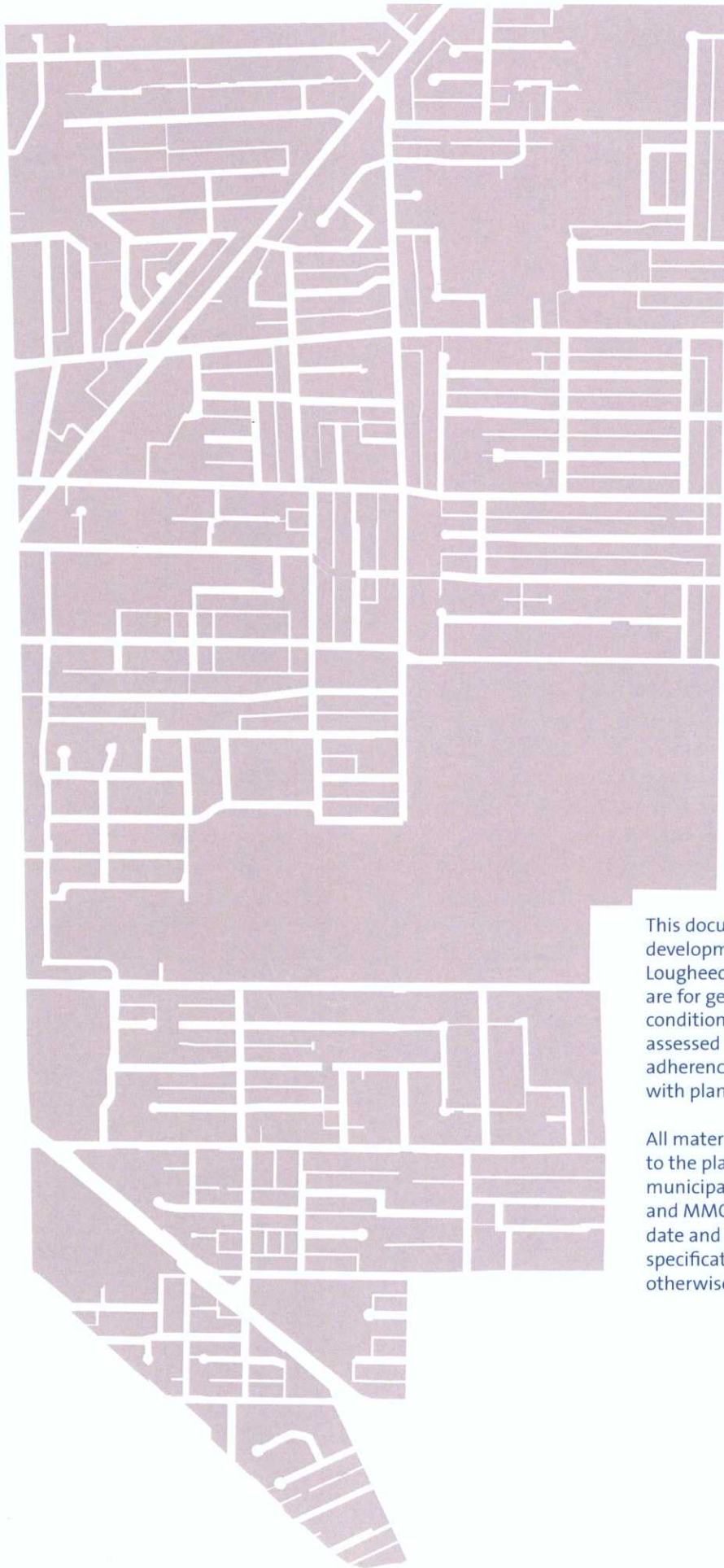




# Burquitlam–Lougheed Neighbourhood Plan *Streetscape Guidelines*

*Shaping growth in  
a neighbourhood of  
neighbourhoods*





This document provides guidelines for development within the Burquitlam - Lougheed Neighbourhood Plan area which are for general guidance only. Site specific conditions will apply and drawings will be assessed by Development Services staff for adherence, completeness and alignment with plan and guideline intent.

All materials and construction shall conform to the platinum edition of the master municipal constructions documents (MMCD) and MMCD supplementary updates to date and City of Coquitlam supplementary specifications and detail drawings unless otherwise noted.



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## EXECUTIVE SUMMARY

This update to the Burquitlam – Lougheed *Streetscape Guidelines* has been prepared to detail the improvements to the overall look and feel of key public areas in Burquitlam – Lougheed. These improvements include on-street parking, improved sidewalks, street furniture, public walkways and plazas, public art, street trees and landscaping.

Design objectives of the Burquitlam – Lougheed Neighbourhood Plan were used to develop specific design guidelines for key areas of Burquitlam – Lougheed.

These include:

- › Strong design elements that identify the Burquitlam – Lougheed Neighbourhood and unify the sense of place within the community with aesthetically attractive art, street furnishing, building design and street frontage / greenway design. They provide a type of neighbourhood ‘branding’ with a unique design vocabulary.
- › Streetscape Design Standards and Zones to ensure that the flow of movement is not impeded by furnishing, that building frontages interact with the street without affecting movement, and ensure smooth transition from sidewalk to crosswalk.
- › Greenway Design Guidelines to guide the development and form of the neighbourhoods’ Citywide Greenways and Neighbourhood Greenways improving the overall pedestrian and cycling experience.
- › Node design guidelines that shape future focal points and gathering points in the neighbourhood. The Node design guidelines will ensure that nodes are prominent and usable natural meeting and resting places throughout the plan area.
- › Site Furnishing Requirements to ensure that as the neighbourhood’s streetscapes are improved, the furnishings, paving materials, lighting, landscaping and public art fit existing neighbourhood character, and build a stronger and more resilient streetscape.
- › The majority of streetscape improvements will be constructed at the time of redevelopment and funded by the developer. A staff interdepartmental working group determined the appropriate scope and level of design that will be required to improve streetscapes in the BLNP.

These *Streetscape Guidelines* will help ensure high-quality urban design, function, and a cohesive, fiscally balanced program.



# 1.0 INTRODUCTION

## 1.1 INTENT

The BLNP Streetscape Guidelines are a companion document to the *Burquitlam – Lougheed Neighbourhood Plan* (BLNP) as adopted by Coquitlam City Council in 2017. The BLNP forms part of the City of Coquitlam’s *Citywide Official Community Plan* (CWOCP). These guidelines are to be used as part of the design and review of public and private development projects within the lands delineated in *Schedule A* of the BLNP.

These guidelines address the design of the following:

- Streetscapes,
- Greenways, and
- Public Open Space.

The guidelines of this document provide specificity to the Vision and policies of the updated BLNP and focus on three critical elements:

- Creation of neighbourhood identity,
- Achievement of livability, and
- Sense of a human scale within a pedestrian-friendly environment.

Except as noted in this document, areas outside the boundaries of where these streetscape guidelines apply are guided by the standard *City of Coquitlam’s Subdivision and Development Servicing Bylaw No. 3558, 2003*, and other City standards and requirements.



*Artist's rendering of a Burquitlam – Lougheed streetscape*



## 1.2 HOW TO USE THESE GUIDELINES

These guidelines are to be used in the design and review of works and services (known as frontage improvements) for streetscapes in key areas of Burquitlam – Lougheed. These guidelines supplement street design standards in the City of Coquitlam *Subdivision and Development Servicing Bylaw*. When beginning a design or review process, use the following progression:

- **Step 1:** Review the Streetscape Design Principles (Section 2.0) to ensure streetscape designs conform to the goal of achieving great streetscapes in Burquitlam – Lougheed.
- **Step 2:** Use **Figure 3** to determine whether a subject property is along a section of street with a dedicated streetscape level, refer to **Figure 9** to determine whether the property is along a Greenway, and finally review **Figure 10** to determine if the subject property is identified as a **node, gateway or landmark site**.
- **Step 3:** Use **Matrix 1 Streetscape Furnishings Palette** to determine furnishing materials and section 4.0 **Site Furnishings** for instructions regarding furnishings - including colour, street trees and landscaping, lighting requirements and public art.

*Evidence shows that people have better health when they live in communities that are designed to support day-to-day healthy choices, such as being physically active, eating healthy food, and engaging in positive social interactions.*

*From planh.ca*

It is possible for applicants/developers to propose streetscape elements beyond the minimum provisions set out for each frontage type, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the Director of Development Services.

Should a conflict exist between the application of the Streetscape elements identified in this document and typical City of Coquitlam standards, these guidelines take precedence.

## 1.3 GUIDING PRINCIPLES & BEST PRACTICE

These *Streetscape Guidelines* support the Burquitlam – Lougheed Neighbourhood community in the following ways:

- Create streetscapes that foster a sense of place and neighbourhood identity with improved urban design, enhancing the look and feel of the area.
- Create streets that contribute to the economic success of local businesses and to the social well-being of residents and revitalization.
- Create streetscapes that maximize the quality of experience, comfort, safety and universal access for pedestrians while maintaining balance between all modal users.
- Create streetscapes that have long-lasting durability, timelessness, and as such are easy to maintain.
- Create quality, universally accessible streetscapes that maximize experience with increased comfort, safety, and function while accommodating all modal users.

*Cultures and climates differ all over the world, but people are the same. They'll gather in public if you give them a good place to do it.*

*Jan Gehl*



## 1.4 DEFINITION OF STREETScape ZONES

For the purpose of these Guidelines, the streetscape is defined as the area in the road right-of-way between the curb (next to vehicle travel lanes) and the property line (or building face where the streetscape treatment extends onto private property).

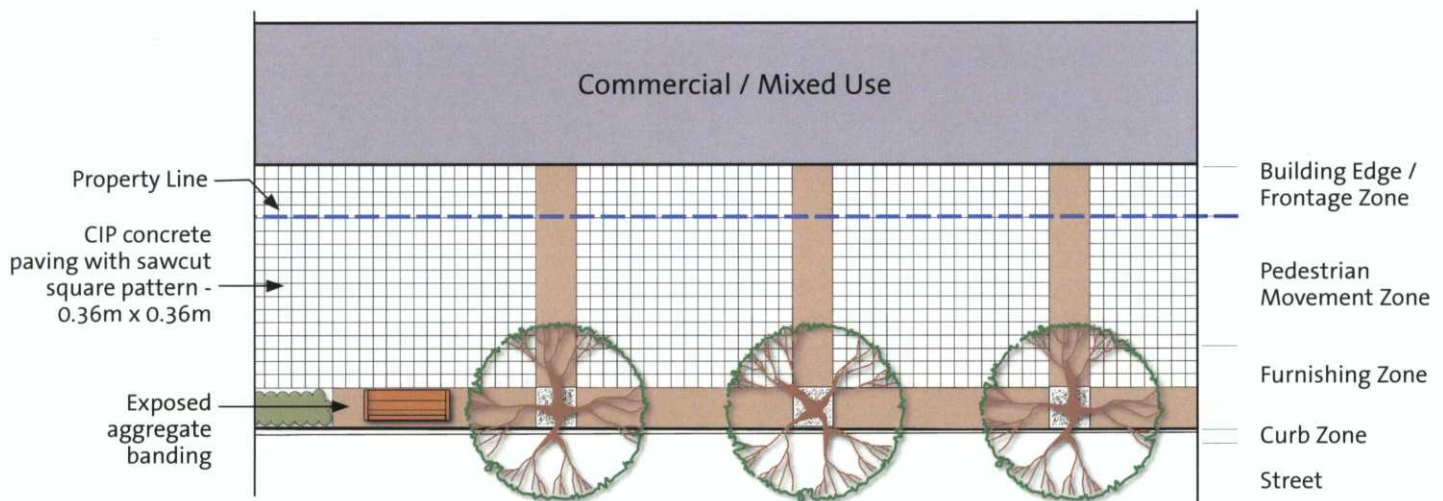
More specifically, the streetscape consists of four zones that serve different functions as illustrated in Figures 1 and 2:

- **Curb Zone**
- **Furnishing or Boulevard Zone** (as determined by adjacent land use);
- **Pedestrian Movement Zone**; and
- **Building Frontage Zone**.

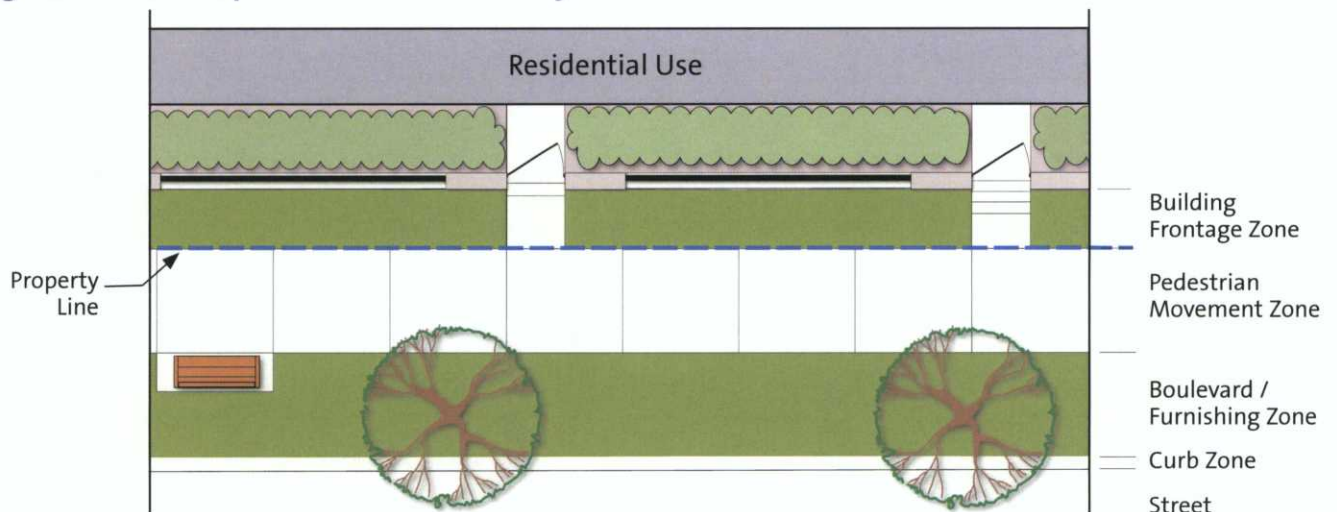
### 1.4.1 Curb Zone

The **Curb Zone** includes barrier or rollover curbs, letdowns for pedestrian and vehicle traffic and gutters to convey rain water to drains and rain gardens where applicable. This zone also accommodates parking pockets in commercial areas.

**Figure 1: Streetscape Zones: Commercial or High Density Residential Uses**



**Figure 2: Streetscape Zones: Lower Density Residential Uses**





### 1.4.2.a Furnishing Zone

*(Applies to Commercial or Higher Density Residential Streetscapes)*

The **Furnishing Zone** is the hard-surfaced area of concrete or pavers adjacent to and between the **Curb Zone** and the **Pedestrian Movement Zone**. It buffers pedestrians from the adjacent roadway and is where streetscape amenities, such as benches, bike racks, streetlights, street trees, landscaping and other elements that could be an obstacle to pedestrian movement are located. Physically separating pedestrians from vehicle travel lanes greatly increases their comfort and this buffering function is especially important on streets where vehicular traffic is heavy, traffic speeds are higher or where on-street parking does not exist.

### 1.4.2.b Boulevard Zone

*(Applies to medium or low density residential Streetscapes)*

In areas of medium or low density residential development, the zone between the **Curb Zone** and the **Pedestrian Movement Zone** is called the **Boulevard Zone**. This strip is adjacent to the curb that buffers pedestrians from the adjacent roadway. In this setting it is typically planted with grass or planting but continues to contain the various street elements that would otherwise impede pedestrian movement. In some instances it can include paving and site furniture such as bike racks or seating.

### 1.4.3 Pedestrian Movement Zone

The **Pedestrian Movement Zone** or “sidewalk” is the area intended for universally accessible pedestrian travel and is free of obstacles, continuous, uniform and predictably located between the **Furnishing / Boulevard Zone** and the **Building Frontage Zone**. This zone also supports universal access, including the use of wheelchairs and strollers. The minimum clearance in the **Pedestrian Movement Zone** should be 2.0 metres.

### 1.4.4 Building Frontage Zone

The **Building Frontage Zone** is the area between the **Pedestrian Movement Zone** and the property line or building face. To promote an active and vibrant street frontage a variety of elements, such as litter receptacles, planter boxes, merchandise displays and café seating, are encouraged within this zone. Vibrancy can be further enhanced where commercial uses such as cafés open onto the street with garage-door style access.

### 1.4.5 Adjoining Streetscapes

Where the street right-of-way is located along the boundaries of a Burquitlam – Lougheed neighbourhood area identified in this document, the opposite side of the street, outside of the Neighbourhood Plan Area, should adopt the higher standard streetscape character as defined by these guidelines. This is to ensure a contiguous streetscape aesthetic and avoid differences from one side of a street to the other.

### 1.4.6 Laneways

Laneways provide critical access for high-density development, but can also offer additional canvas for passive streetscaping. Where appropriate, development should be encouraged to include some laneway programming to complement the neighbourhood character.

*The minimum clearance between street furniture and infrastructure in the Pedestrian Movement Zone will be 2.0 metres, to allow two people to walk or wheel side by side.*



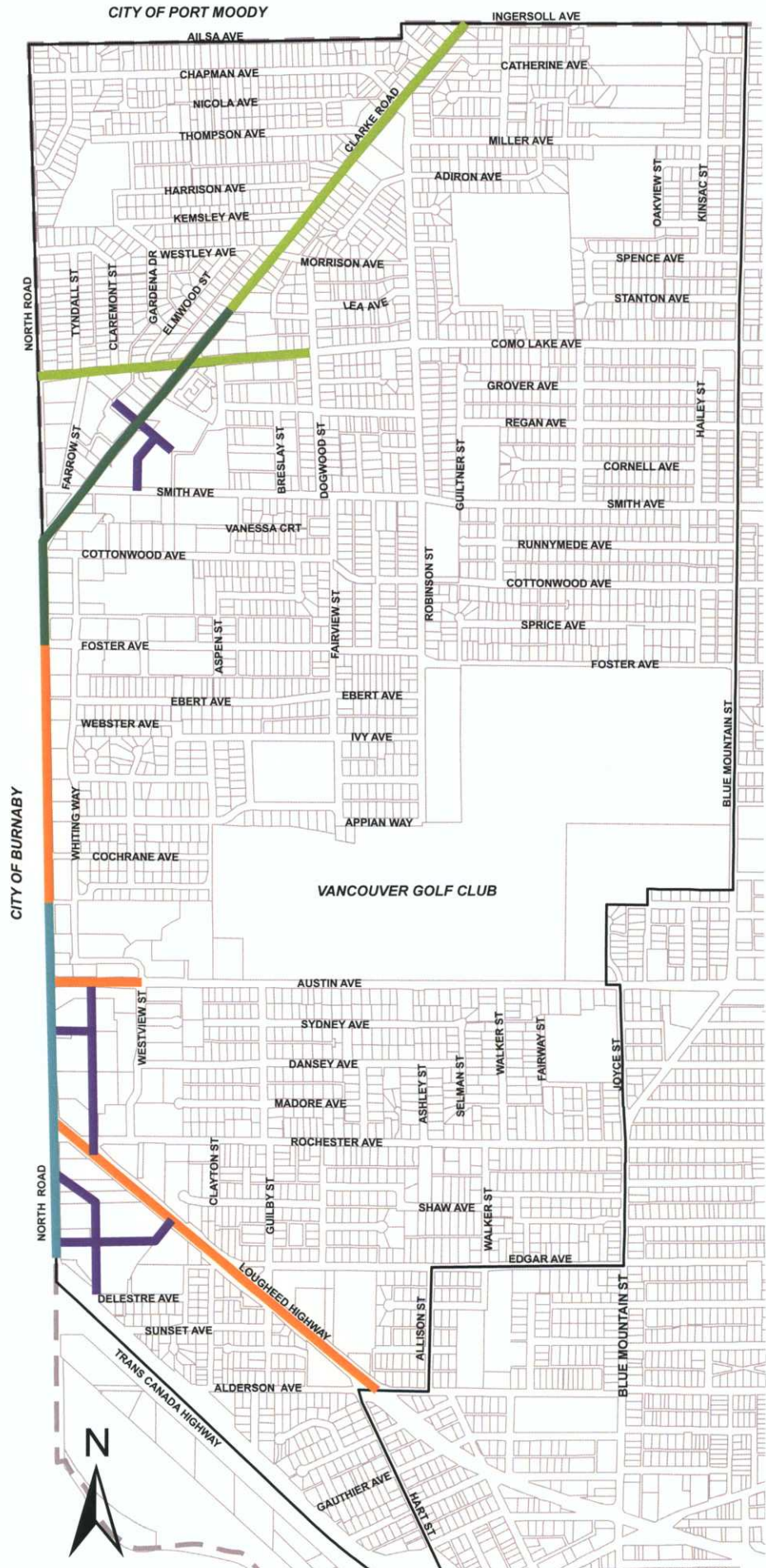
**Figure 3: BLPN Streetscape Hierarchy**

**Legend**

-  Level 1A - North Road - Commercial Frontage
-  Level 1B - Clarke Road - Commercial Frontage
-  Level 2A - North Road (et al) - Residential Frontage
-  Level 2B - Clarke Road (et al) - Residential Frontage
-  Level 3 - Peripheral Commercial Frontage
-  City Boundary
-  Burquitlam – Lougheed Neighbourhood Plan Boundary

**Notes:**

- All other streets will conform to the City of Coquitlam’s *Subdivision and Development Servicing Bylaw No. 3558, 2003*.
- For frontages along North Road, these guidelines pertain to the lands within Coquitlam city limits only.





## 2.0 STREETScape DESIGN PRINCIPLES

### 2.1 STREETScape HIERARCHY LEVELS

The streetscape hierarchy is comprised of 'levels', as illustrated in **Figure 3**.

Streets that are not identified as being **Levels 1-3** in **Figure 3** will defer to the standard established in the city of Coquitlam *Subdivision and Development Servicing Bylaw No. 3558, 2003*.

Specifications on the applicable paving materials lighting, landscaping and site furnishings identified in each 'Level' are provided in section **4.0 Site Furnishings**.

### 2.2 ALL STREETScape LEVELS

- a) Provide enhanced streetscape infrastructure for banners and seasonal lighting receptacles on the street light poles.
- b) Patio and outdoor display areas are encouraged for all **Commercial Frontage** types. In addition, storefronts and café fronts with 'garage door' style openings can help achieve vibrancy.
- c) Uses in the **Building Frontage Zone** could include public seating, outdoor eating associated with restaurants and coffee shops and outside display of merchandise provided that the dimensions of other **Zones** are maintained and the objectives of the City of Coquitlam's *Encroachment Policy* are upheld.
- d) The **Building Frontage Zone** may be used as a secondary siting area for elements of the streetscape that are usually in the **Furnishings Zone**. Where feasible, the minimum private property offset will be increased to allow commercial uses to "spill" onto the sidewalk without impeding the movement zone.
- e) Ventilation structures, controller and other utility vaults, boxes and kiosks should be located underground, their tops flush with the paved surface. Covers, hatches and grates should have a surface texture that is slip-resistant.
- f) Include concrete bands or expansion joints along property lines in hard surface areas.
- g) Align jointing patterns with the outside edges of the planters and the seating pockets.





## 2.3 STREETScape LEVELS 1A & 1B

**Streetscape Levels 1a** and **1b** apply to commercial-fronting portions of North Road and Clarke Road, as indicated in **Figure 3**, where the highest density of mixed-use development occurs. These streetscapes are urban in character and accommodate a strong pedestrian presence.

- › Pedestrian Crosswalks will be wider than the Citywide standard to accommodate higher numbers of users around Evergreen Line stations.
- › The **Pedestrian Movement Zone** will be at least 3.0 metres wide.
- › The **Furnishing Zone** will be at least 2.5 metres wide.



## 2.4 STREETScape LEVELS 2A & 2B

**Streetscape Levels 2a** and **2b** apply to portions of Lougheed Highway, Austin Avenue, Como Lake Avenue and North Road located on the perimeter of areas with High Density multiple-family development. They are urban in character and accommodate a strong pedestrian presence. In addition, this streetscape has a 'supportive' functional significance in the public realm.

- › Pedestrian Crosswalks will be wider than the Citywide standard where sites allow to accommodate higher numbers of users around Evergreen Line stations. These sidewalks will narrow to the citywide standard at sidewalk let-downs and crosswalks .
- › The **Pedestrian Movement Zone** will be at least 2.0 metres wide.
- › The **Furnishing Zone** will be at least 2.5 metres wide.



## 2.5 STREETScape LEVEL 3

**Streetscape Level 3** applies to heavy traffic commercial areas that radiate outwards from the dense centre and are typically adjacent to residential high and medium density development. They are urban in character and accommodate a moderate pedestrian presence, where commercial use represents the best use of at-grade space.

Figure 4: Streetscape Level 1a: Commercial Frontage

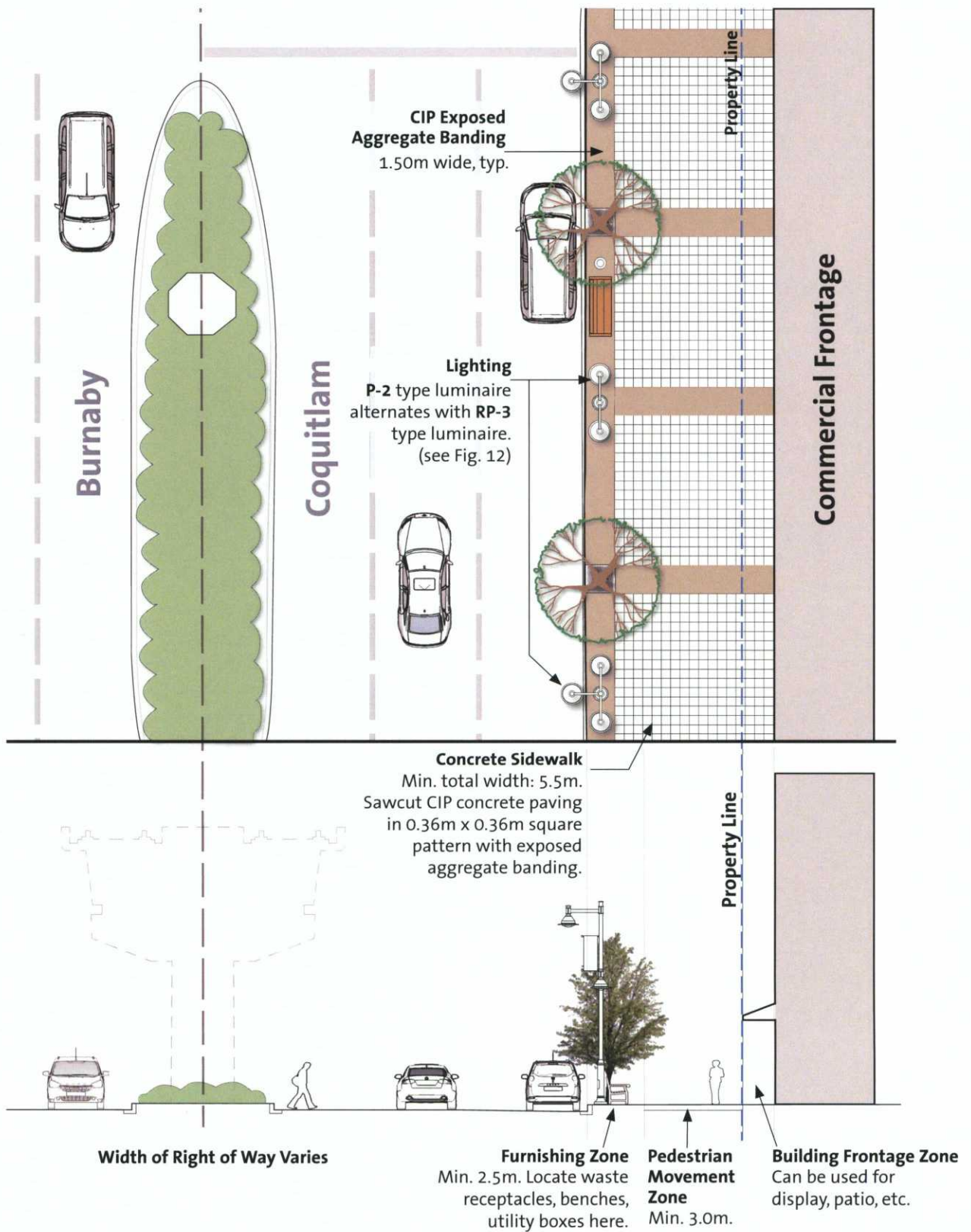


Figure 5: Streetscape Level 1b: Commercial Frontage

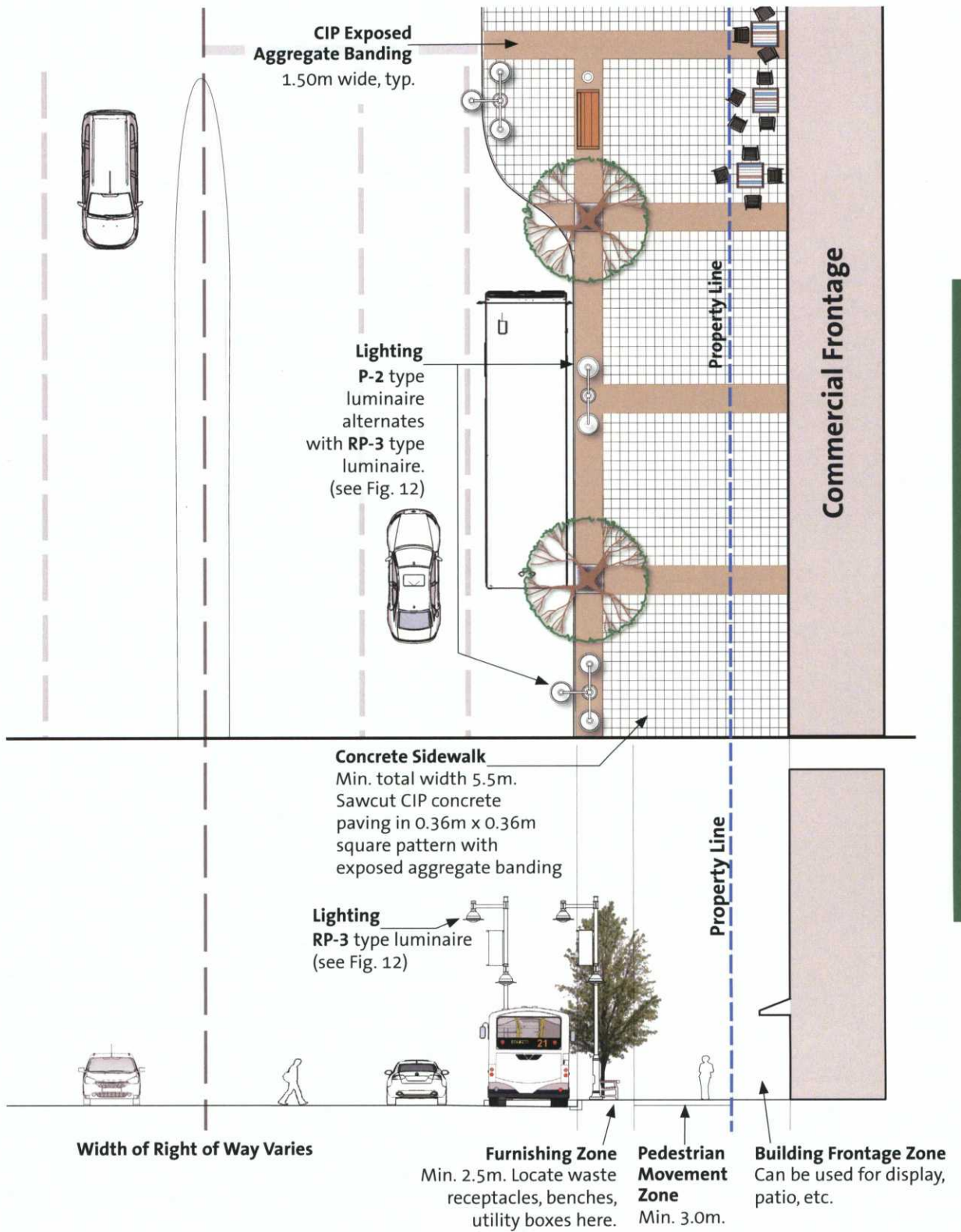




Figure 6: Streetscape Level 2a: Residential Frontage

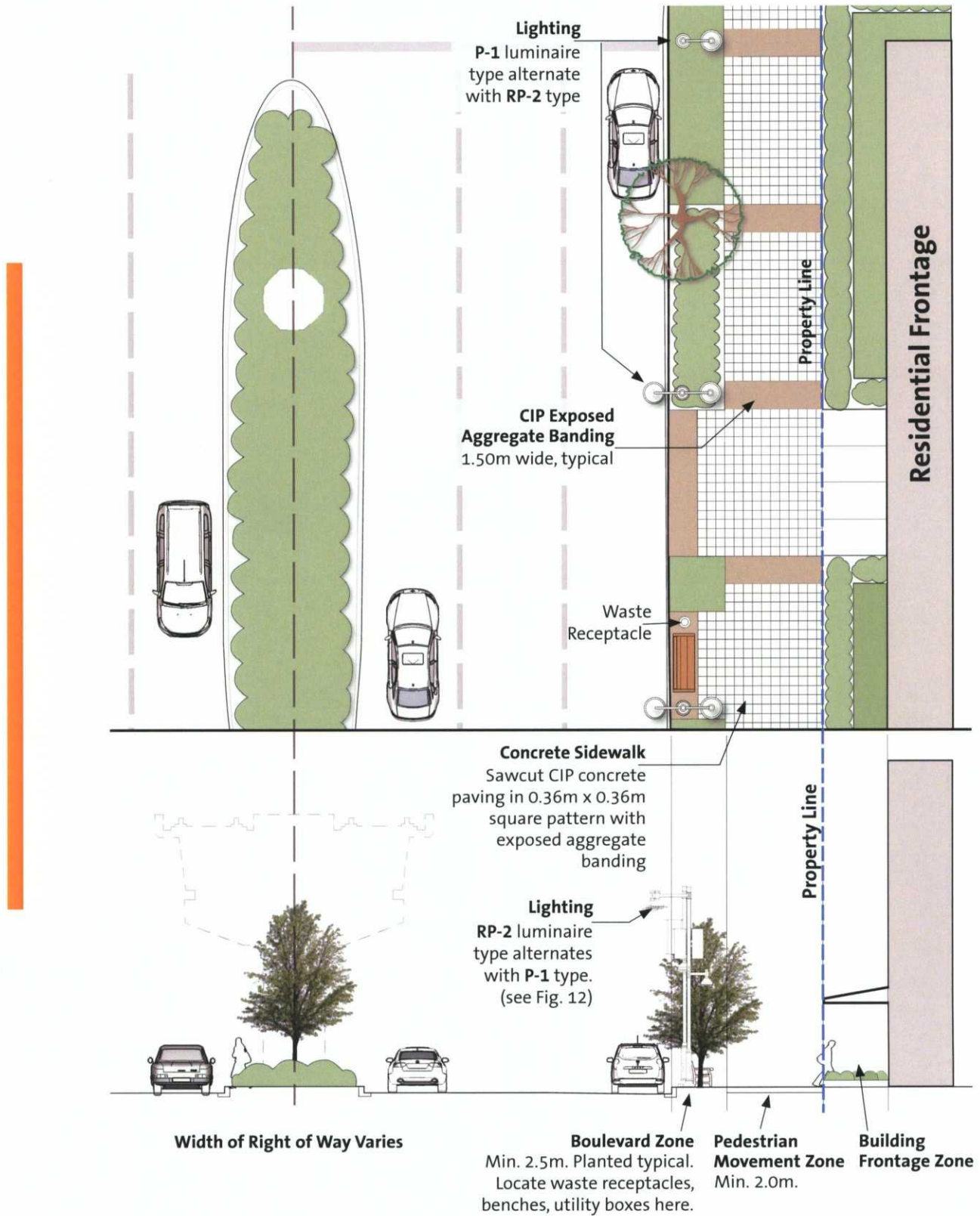


Figure 7: Streetscape Level 2b: Residential Frontage

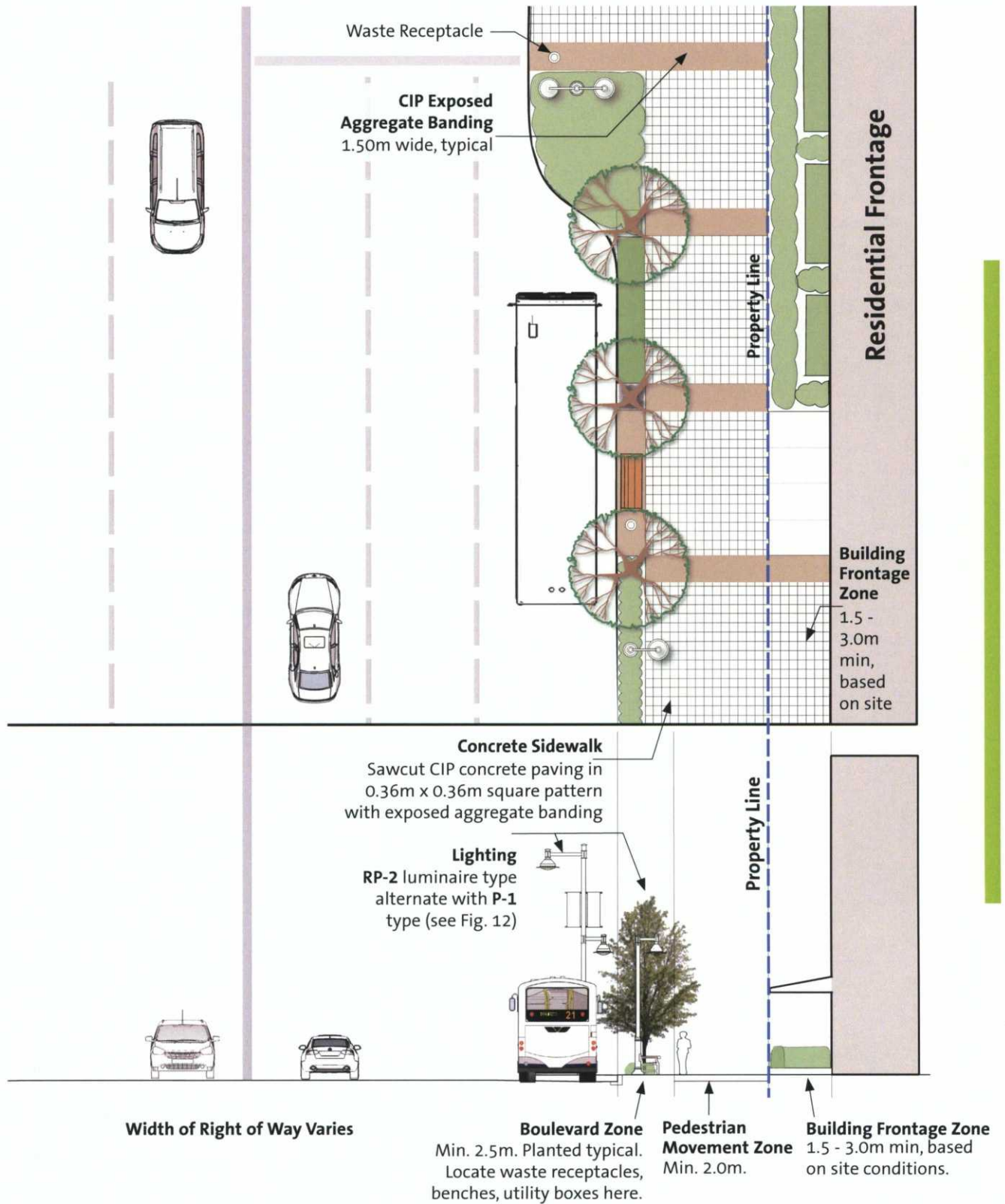




Figure 8: Streetscape Level 3: Commercial Frontage

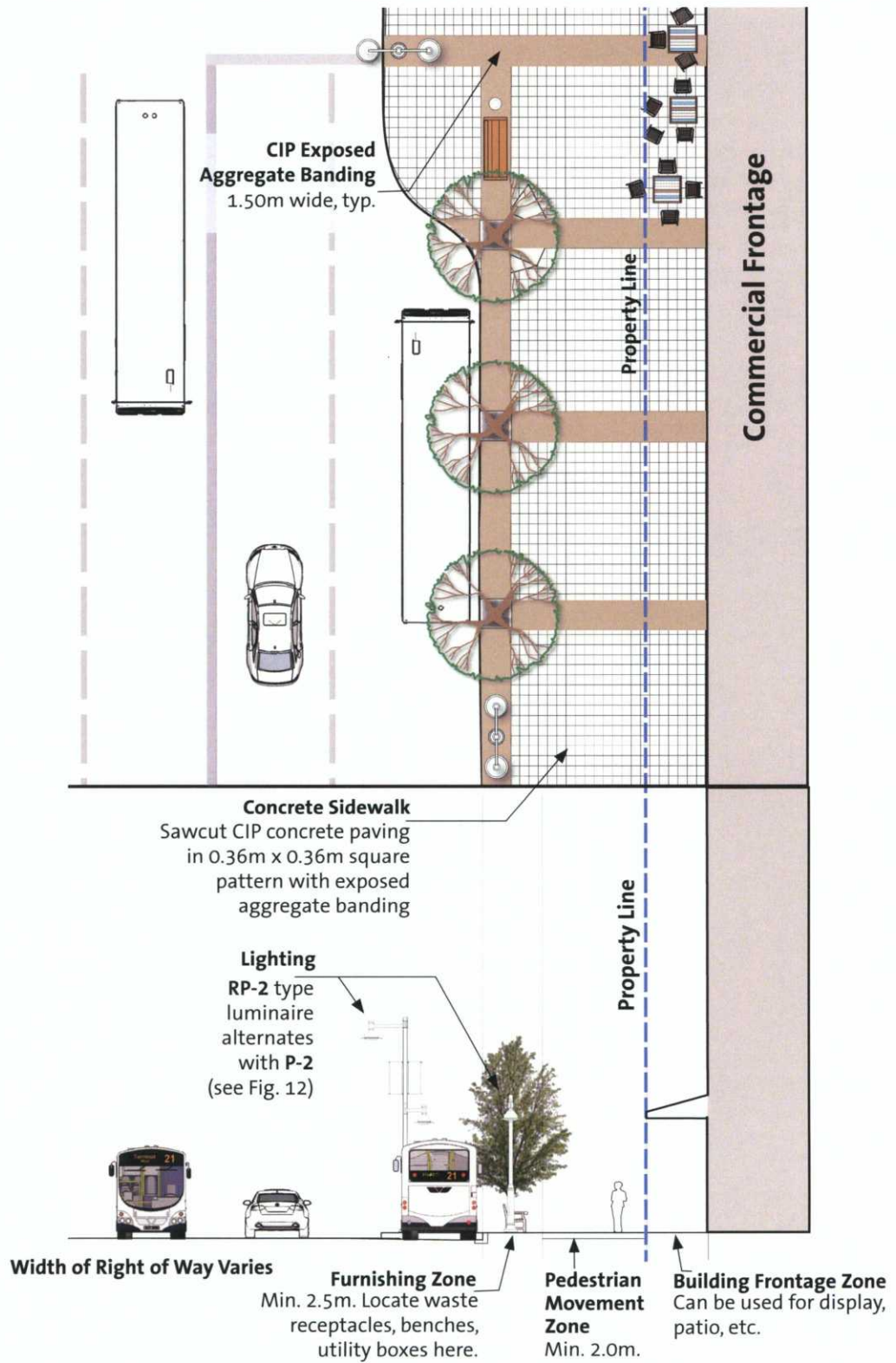




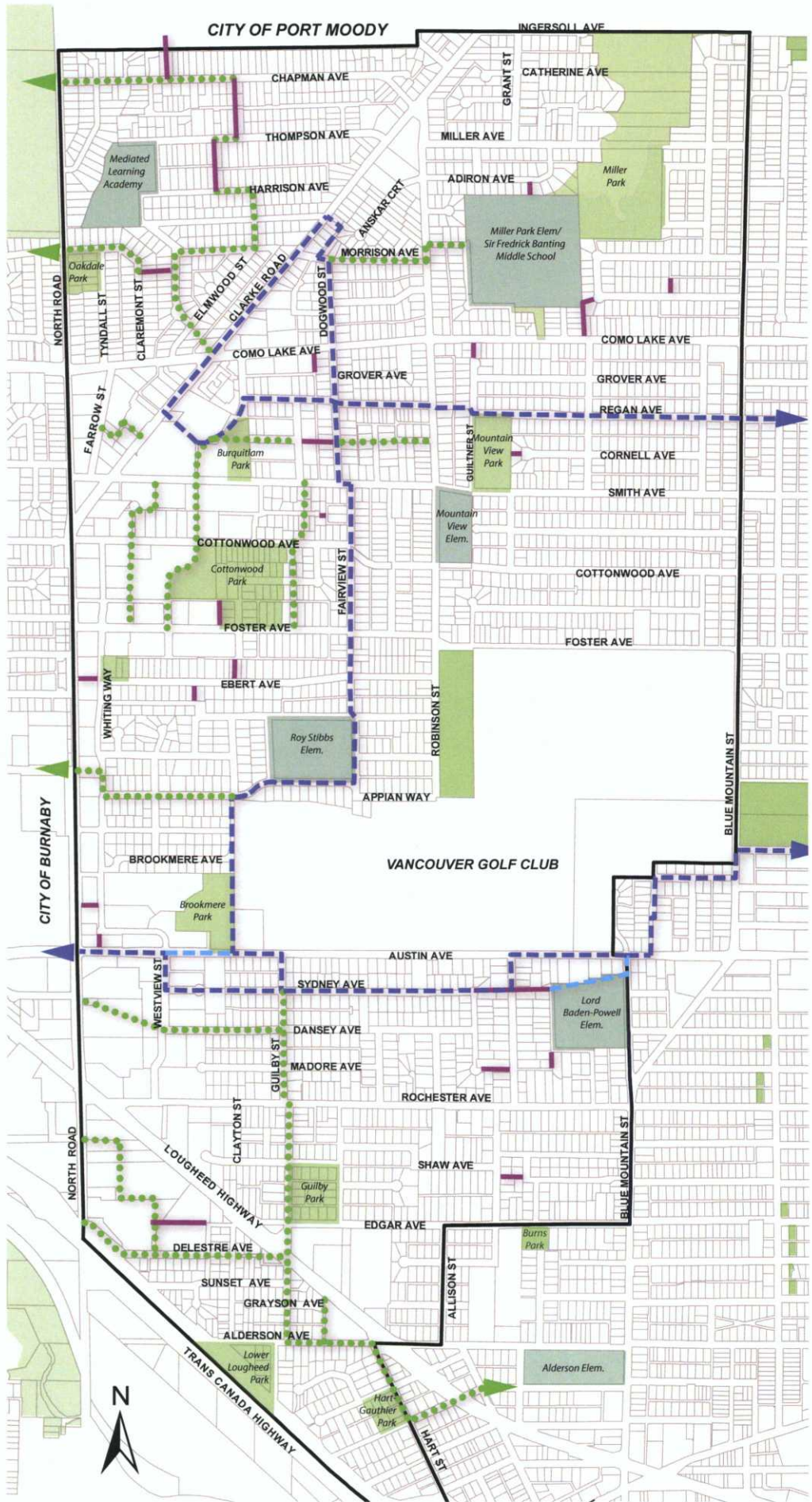
Figure 9: Greenway Network

**Legend**

-  Proposed Citywide Greenway
-  Proposed Citywide Greenway Alternate
-  Proposed Neighbourhood Greenway
-  Existing Walkway
-  Parks
-  Schools
-  Burquitlam – Lougheed Neighbourhood Plan Boundary

**Notes:**

- Citywide Greenways will be built by the city.
- Neighbourhood Greenways will be built together with adjacent redevelopment.
- Citywide Greenways are identified in Coquitlam’s *Strategic Transportation Plan* and are intended to be continuous routes that link major destinations throughout the city, including major commercial centres, schools, parks, amenities, and SkyTrain stations.
- Three major citywide Greenways have been identified:
  - **Dogwood-Fairview Greenway**
  - **Smith-Regan Greenway**
  - **Austin-Sydney Greenway**



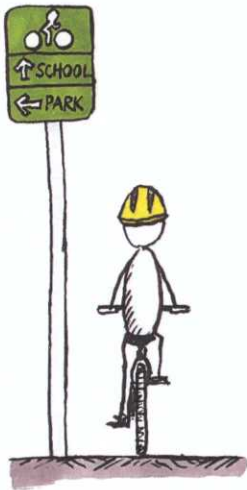




## 3.0 GREENWAY AND NODE DESIGN

### 3.1 GREENWAYS

Coquitlam's *Strategic Transportation Plan* (2012) includes a comprehensive network of **Citywide Greenways** which will be funded by the City as capital projects from Development Cost Charges (DCCs). This is supported and further connected by a network of **Neighbourhood Greenways** in the BLPN area which will be funded as off-site improvements during redevelopment. Greenways are intended to be multi-modal corridors that encourage and support walking and cycling for both recreational and commuting purposes.



These guidelines establish a distinct aesthetic and character to identify multi-modal routes which differ from the standard sidewalk. Furthermore, this visual difference helps to promote non-vehicle movement within Burquitlam – Lougheed and the City of Coquitlam. The unique character also helps to identify to cyclists and pedestrians that they have arrived in Burquitlam – Lougheed from adjacent portions of the City.

#### 3.1.1 Citywide Greenways

**Citywide Greenways** are intended to be continuous routes that strategically link major destinations throughout the City, including major commercial centres, schools, parks, and other community facilities, as shown in **Figure 9**. The streetscape improvements that accompany **Citywide Greenways** are generally funded through Development Cost Charges.



*Greenways can take a variety of forms, depending on the local context*



### 3.1.2 Neighbourhood Greenways

**Neighbourhood Greenways** are generally shorter and provide connections within the City Centre and Neighbourhood Centres, as well as connections to the **Citywide Greenway** network and new and enhanced connector pathways, where opportunities exist, as an opportunity to enhance pedestrian connections and shorten walking distances. **Figure 9** shows the **Neighbourhood Greenways** in the Burquitlam – Lougheed Neighbourhood.

**Neighbourhood Greenways** are paid for by frontage improvements that accompany individual site redevelopment projects and are completed at that time.

The City supports the development of a network of **Citywide** and **Neighbourhood Greenways** made up of both on-street and off-street facilities throughout the community. Enhanced treatments distinguish these greenways from other cycling and walking routes. Potential treatments along **Citywide** and **Neighbourhood Greenways** are shown in **Table 1**.

The specific design treatments for individual greenways will be determined through functional design at the time of their development.

**Table 1: Greenway Design Elements**

Feature	Local Streets	Collector and Arterial Streets	Off-Street
Enhanced sidewalk width	✓	✓	
Local street bikeway (on low volume roadways)	✓		
Multi-use pathway - MUP (continuous, accessible path on one side of the street)		✓	✓
Additional boulevard planting	✓	✓	✓
Narrower crossings (curb extensions)	✓	✓	
Traffic calming (discourage speeding and short-cutting)	✓		
Street furniture (e.g., park benches and water fountains)		✓	✓
Pedestrian lighting		✓	✓
Public art (interpretive signage)	✓	✓	✓
Alternative stormwater management	✓		

**Notes:** Curb bulges on arterials will only be considered in conjunction with parking pockets, or where all mobility / capacity needs are met.



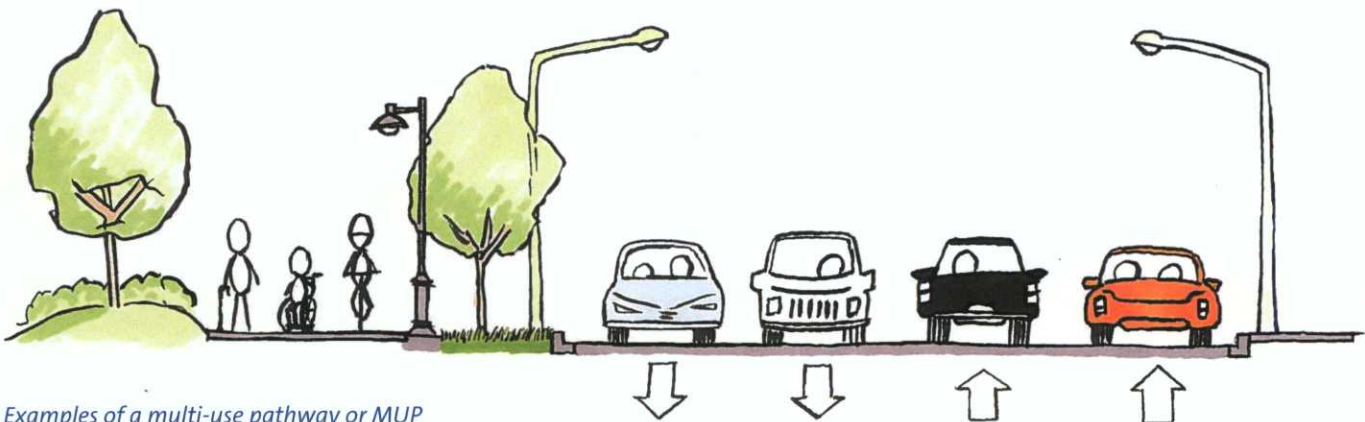


### Design intent:

- To have majestic, long-lived and healthy street trees throughout the Burquitlam – Lougheed neighbourhood;
- To have long-lived and healthy large-scale street trees;
- To have street trees that address storm water management, biodiversity, and that offer great environmental benefits;
- To create a unique sense of place for Burquitlam – Lougheed within the greater city context;
- To provide pleasant places for walking and cycling.

### Guidelines:

- Develop a clear visual cue to identify **Greenways** as a multi-modal route within the neighbourhood.
- Establish a uniform treatment of **Greenway** surfacing and layout to reinforce the use and character.
- Regularly spaced **nodes** provide rest areas at the top of hills to support a diversity of ages and abilities.
- Create **nodes** with layouts and key elements reflective of Burquitlam – Lougheed’s history.



Examples of a multi-use pathway or MUP

## 3.2 NODES

**Nodes** are strategic focal points for orientation like squares and junctions. As they are typically located along transportation routes, they are suitable locations for a public resting or gathering place. They are usually located at intersections along streets or **Greenways**. **Node** locations are identified on *Schedule E* of the *BLNP* and are shown in **Figure 10**.

**Nodes** form an integral part of the **Greenway** network by supporting neighbourhood and city-wide walkability. They provide a place to sit for seniors and children in a public plaza or green space. They represent an important informal place in the community.

### Node dimensions:

- › **Primary Nodes** include a public gathering space of at least 250m<sup>2</sup>.
- › **Major Nodes** will be defined by a plaza of at least 45m<sup>2</sup>.
- › **Minor Nodes** will include a plaza or green space of at least 16m<sup>2</sup>.

### 3.2.1 Primary Nodes - Requirements & Guidelines

Burquitlam Neighbourhood Centre and Lougheed Neighbourhood Centre are identified as **Primary Nodes**. They are unique and important commercial precincts for the Burquitlam – Lougheed area. North Road and Clarke Road will have increased residential densities, resulting in higher demands on the streetscape corridor (see **Figure 3**).

As illustrated in **Figure 10**, a **Citywide Greenway** bisects these Neighbourhood Centre areas. Design requirements identified for the two Neighbourhood Centres and the North Road / Clarke Road Corridor will need to be coordinated and integrated with the future implementation of the **Citywide Greenway**.

**Primary Nodes** provide a public open space that is a minimum of 250m<sup>2</sup> and shall include elements such as:

- › A portion of weather protection coverage for year-round use;
- › Seating and other appropriate street furniture including bicycle storage;
- › Public art of a scale that highlights the significance of the space;
- › A mix of hard and soft landscaping;
- › Pedestrian oriented lighting that can illuminate the space for evening programming;
- › Wayfinding;
- › Fronting buildings will have active frontages.

### Guidelines:

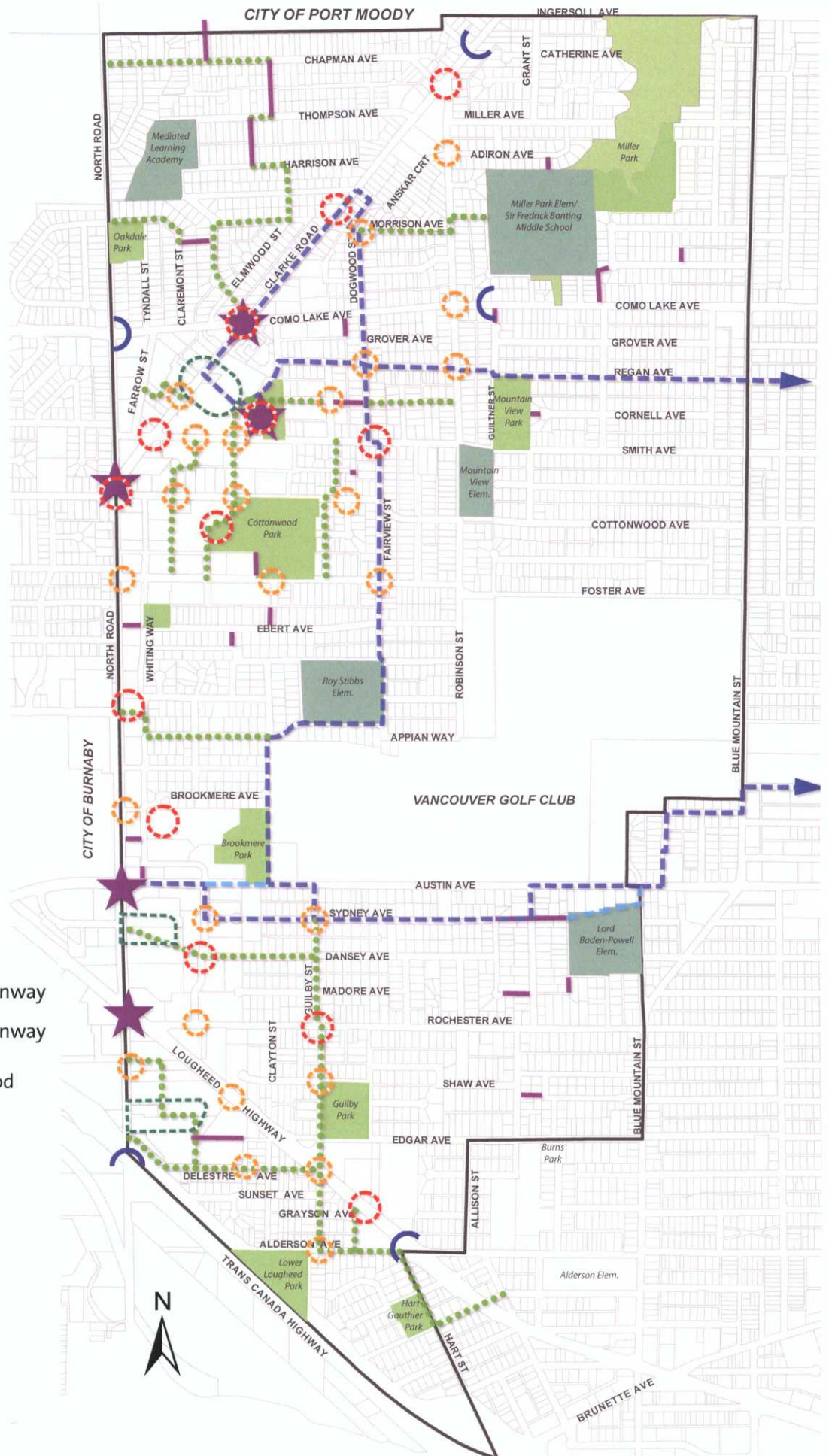
- a) Ensure layout and key elements are reflective of Burquitlam and Lougheed history;
- b) Provide streetscape infrastructure for banners and seasonal lighting on the light poles;
- c) Locate infrastructure for banners on buildings or building frontages;
- d) Provide increased sidewalk width beyond the minimum standard and ensure smooth pedestrian routes through the node.



Figure 10: Landmark, Node and Gateway Locations

### Legend

-  Primary Node
-  Major Node
-  Minor Node
-  Landmark
-  Gateway
-  Proposed Citywide Greenway
-  Proposed Citywide Greenway Alternate
-  Proposed Neighbourhood Greenway
-  Existing Walkway
-  Parks
-  Schools
-  Burquitlam – Lougheed Neighbourhood Plan Boundary



### 3.2.2 Major Nodes

**Major Nodes**, as identified in **Figure 10 / Schedule E** of the BLNP, will provide a public open space that is a minimum of 45m<sup>2</sup>. **Major nodes** encourage public gathering and lingering and shall include elements such as:

- Seating and other appropriate street furniture;
- Public art;
- A mix of hard and soft landscaping;
- Pedestrian oriented lighting;
- Wayfinding;
- Fronting buildings shall address the node with active frontages, architecturally distinctive façades, and high-quality streetscape treatments; and,
- Where node spaces are adjacent to commercial retail units (CRUs), cafés, coffee shops and ice cream parlours are encouraged.

#### Guidelines:

- a) Provide streetscape infrastructure for banners and seasonal lighting on the light poles per **Matrix 1**.
- b) Locate infrastructure for banners on buildings or building frontages.
- c) Provide increased sidewalk width beyond the minimum standard along exterior frontages where the width of the right of way can accommodate it.
- d) Plant boulevards according to the preferred boulevard plan and street tree lists and in coordination with City Urban Forestry and Parks Planning staff. Particular emphasis should be placed on integrating plants as a prominent character element on all frontages.

### 3.2.3 Minor Nodes

Development located adjacent to **Minor Nodes**, as identified on **Figure 10**, is to provide a public open space that is a minimum of 16 m<sup>2</sup>. **Minor nodes** will be designed to encourage public gathering and lingering and shall include elements such as:

- Seating and other appropriate street furniture;
- Public art;
- A mix of hard and soft landscaping; and
- Fronting buildings shall address the **node** with active frontages, architecturally distinctive façades, and high-quality streetscape treatments.

#### Guidelines:

- a) Provide increased sidewalk width beyond the minimum standard where width of right of way can accommodate it.





### 3.3 GATEWAYS

**Gateways**, as identified in **Figure 10 / Schedule E** of the BLNP, are distinct public places that represent ideas or events that are important to the community.

**Gateways** are ideal locations for public art and memorials to historic people and events and enhanced public realm design.

**Gateways** should include elements such as:

- › Public art;
- › Signage;
- › Unique public spaces and plazas;
- › Distinctive lighting;
- › Distinctive building facades;
- › High quality site furnishing;
- › Improved street presence.

*“In lively, safe, sustainable and healthy cities, the prerequisite for city life is good walking opportunities ... a multitude of valuable social and recreational opportunities naturally emerge when you reinforce life on foot.”*

– Jan Gehl, *Cities for People*

### 3.4 LANDMARK SITES & BUILDINGS

**Landmarks** are external points of orientation, usually an easily identifiable physical object in the urban landscape. They are ideal locations for public art.

Buildings located at **landmark** sites and/or over 30 storeys in height will have a greater impact on the neighbourhood aesthetic character and will be required to achieve a higher level of design excellence. They are required to have enhanced public realm design. A rigorous design review process for these sites and buildings is outlined in the BLNP, section 4.3.

**Landmark** sites are identified in **Figure 10 / Schedule E** of the BLNP, whereas landmark buildings are either buildings located at landmark sites or buildings greater than 30 storeys in height.



*High quality public spaces include safe places to rest, supporting walkable neighbourhoods for residents of all ages*



## 4.0 SITE FURNISHINGS

Matrix 1: Street Furnishing Palette

Feature	Specifications	Streetscape		
		Level 1a & 1b	Level 2a & 2b	Level 3
<p><b>Benches</b></p>  <p><b>Level 1</b> - benches include medallion <b>Level 2 and 3</b> - no medallion</p>	<p>Metal framed bench with wood slat seat and back;</p> <p><b>Supplier:</b> Frances Andrew Type: G24-311 <b>Model:</b> Garden Series Bench <b>Colour:</b> Black with Ipe Hardwood slat <b>Material:</b> Standard Aluminum Cast <b>Finish:</b> Polyester Powder Coating <b>Supplier:</b> Frances Andrews Site Furnishings Ltd, Surrey, BC <b>Sustainability:</b> Recycled content</p>	<p>With medallion Minimum four per block.</p>	<p>No medallion Minimum four per block.</p>	<p>No medallion Two located at major node.</p>
<p><b>Garbage Receptacles</b></p> 	<p>Steel receptacle with vertical slats;</p> <p><b>Supplier:</b> Frances Andrew 'Carriage Lane' Series <b>Type:</b> R-31 <b>Colour:</b> Black <b>Installation:</b> per manufacturer's specifications or approved equivalent;</p>	<p>minimum two per block</p>	<p>minimum two per block</p>	<p>minimum one per block</p>
<p><b>Bollards</b></p> 	<p>Powder coated carbon steel;</p> <p><b>Supplier:</b> Reliance Foundry <b>Type:</b> R-7901 <b>Height:</b> 36in <b>Weight:</b> 38lbs <b>Colour:</b> Black <b>Installation:</b> various available - concrete embedment, bolt-down flange, removable receivers and fold-down mountings. <b>Accessories:</b> Variety of chain links and padlock</p>	<p>where required</p>	<p>where required</p>	<p>where required</p>
<p><b>Bike Racks</b></p> 	<p><b>Supplier:</b> Landform <b>Model:</b> Bola <b>Colour:</b> Stainless Steel <b>Material:</b> Stainless Steel <b>Finish:</b> Powder coat <b>Sustainability:</b> Recycled content, Recyclable <b>Installation:</b> embedded <b>Leed 2009:</b> SS Credit 4.2 Alternative Transportation—Bicycle Storage and Changing Rooms / MR Credit 4, Recycled Content / MR Credit 5, Regional Materials</p>	<p>at least two per block</p>	<p>at least one per block</p>	<p>at least one per block</p>
<p><b>Fencing</b></p>	<p>Fencing required to control pedestrian movement should be painted metal in rectilinear forms with vertical dominating over horizontal (non-climbable).</p>	<p>where required</p>	<p>where required</p>	<p>where required</p>
<p><b>Drinking Fountains</b></p>	<p>Optional, sturdy and vandal resistant</p>	<p>where required</p>	<p>where required</p>	<p>where required</p>



Feature	Specifications	Streetscape Level				
		1a	1b	2a	2b	3
<b>Roadway and Pedestrian Lighting</b> 	<p>Philips Lumec Transit Series configurations as illustrated in <b>Figure 12</b>.</p> <p>To be coordinated with pedestrian lighting.</p> <p>Include banner arm pairs for commercial frontage streetscapes.</p> <p>Luminaire: TR20  <b>Lamp Type:</b> LED  <b>Mounting:</b> SN  <b>Finish:</b> Powder coat  <b>Colour:</b> Black  <b>Base:</b> SM</p>	Type RP-3 alternate with type P-2	Type RP-2 alternate with type P-1	Type RP-2 alternate with type P-2		
		<b>Include Banner Arms and Duplex Receptacles on pole?</b>				
		✓	X	✓	X	✓
<b>Tree Grate</b> 	<p><b>Supplier:</b> Dobney Foundry, Surrey, BC  <b>Type:</b> ST48  <b>Colour:</b> Black  <b>Material:</b> Ductile Iron  <b>Finish:</b> Unfinished or Oiled  <b>Weight:</b> 240 lbs approximate  <b>Sustainability:</b> Recycled content, recyclable  <b>Installation:</b> Concrete surround with 48" tree grate frame</p>	✓	X	✓	X	✓

**Notes:**

The locations and frequency of site furnishing elements are to be determined during functional design on a case - by - case basis at the time of redevelopment. As a rule, the quantity and location of these furnishings should be generously applied, as reasonable and appropriate, and in concurrence with other perimeter works and services and as determined by the General Manager Planning and Development.

Developers may propose additional streetscape design components beyond the minimum provisions identified for each frontage type, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager Planning and Development.

## 4.2 Paving Materials

All paving in publicly accessible areas should be chosen with regard for maximum accessibility and safety for those with mobility impairments, wheelchairs, canes, walkers, strollers and bicycles.

### Materials

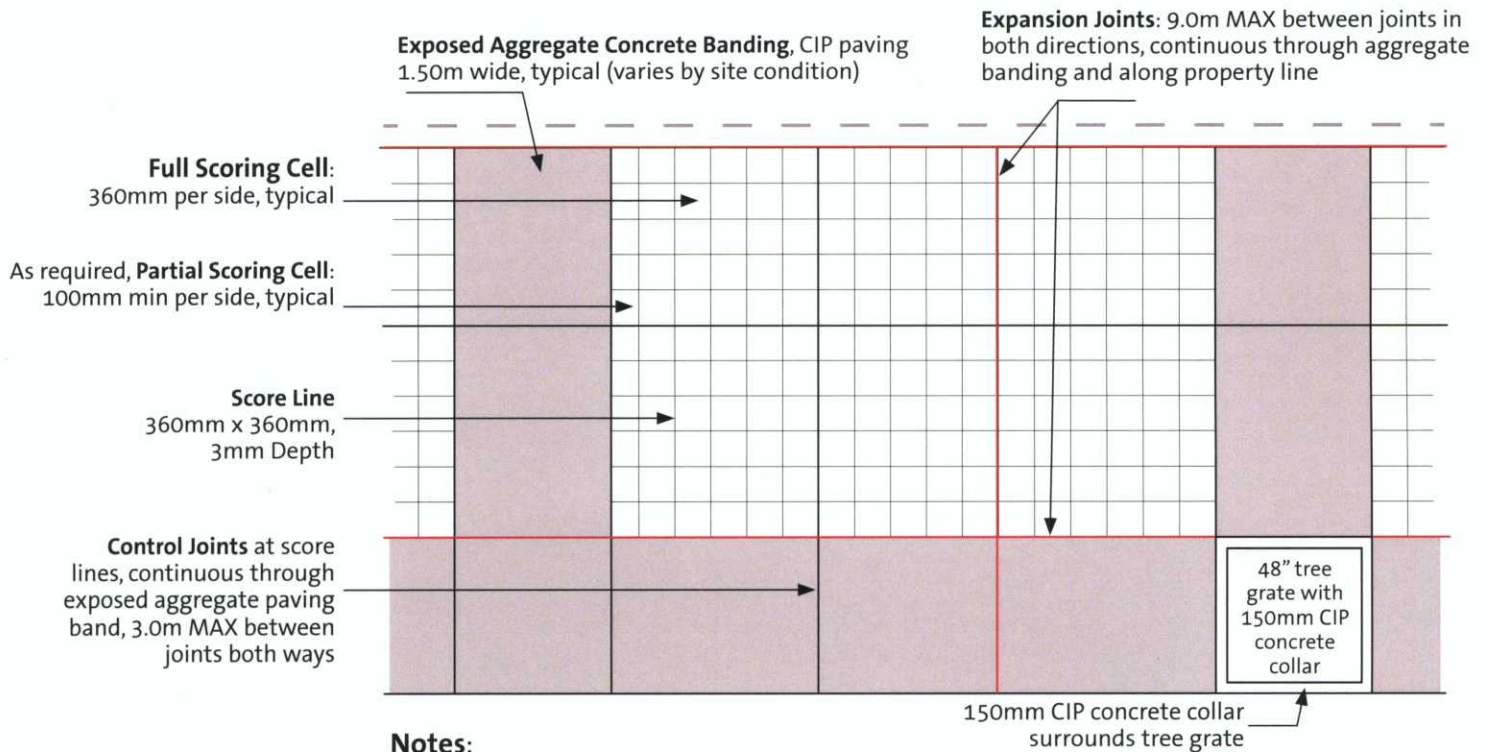
Simple paving finishes of scored natural and coloured concrete are used within the Neighbourhood Centres to allow a strong visual context for Burquitlam – Lougheed as illustrated on **Figure 11**.

The **Pedestrian Movement Zone** has a simple surface with subtle textural changes helping to identify seating areas in **Nodes**.

**Figure 11: Concrete Paving Pattern**

### Plan

Drawing not to scale



### Notes:

1. Joint pattern may vary slightly with reduced widths of banding.
2. "Scoreline" is a 3mm x 3mm dimension joint. They are to be saw-cut. Trowelling will not be accepted.
3. Tree grate - Dobney Foundry ST48 - see **Matrix 1**.





## 4.3 Lighting

Lighting elements will reflect the features of the existing streetscape and build upon that aesthetic with currently available, energy efficient and durable poles and current technology luminaire options. The lighting products listed in this section will be applied to the **Streetscapes Levels 1, 2, and 3** as shown in **Figures 4 - 8**.

Recommended hardware for **Streetscape Levels 1, 2 and 3** is:

- ▶ Philips Lumec Transit series TR-20 luminaires and poles with powder coat finish in semi-gloss textured black.

Poles located on all commercial street frontages as shown in **Figure 3** will:

- ▶ accommodate banners as specified by the City of Coquitlam. Poles in these areas will be equipped with banner accessories.
- ▶ come equipped with exterior rated GFCI plugs to enable seasonal lighting installations. A guy wire system at a minimum elevation of 2.4m should be provided between poles without the plugs to support low voltage lighting extensions.
- ▶ Roadway lighting should be paired with pedestrian lighting wherever possible.

**Figure 12** shows preferred configurations of luminaires and poles. Developers may propose additional streetscape design components beyond the minimum provisions identified for each frontage type, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager Planning and Development.

### 4.3.2 Roadway Lighting

#### Guidelines:

Some example configurations are provided in **Figure 12** but developers should work out the correct spacing, arm lengths and fixture height based on industry standard modeling and requirements, BC Building Code, road classification, and City of Coquitlam requirements. The spacing of the light standards will be optimized to reduce impacts on street trees.

*An example of Lumec Transit series Roadway / Pedestrian configuration type RP-3 with banner hardware*



### 4.3.3 Pedestrian Lighting

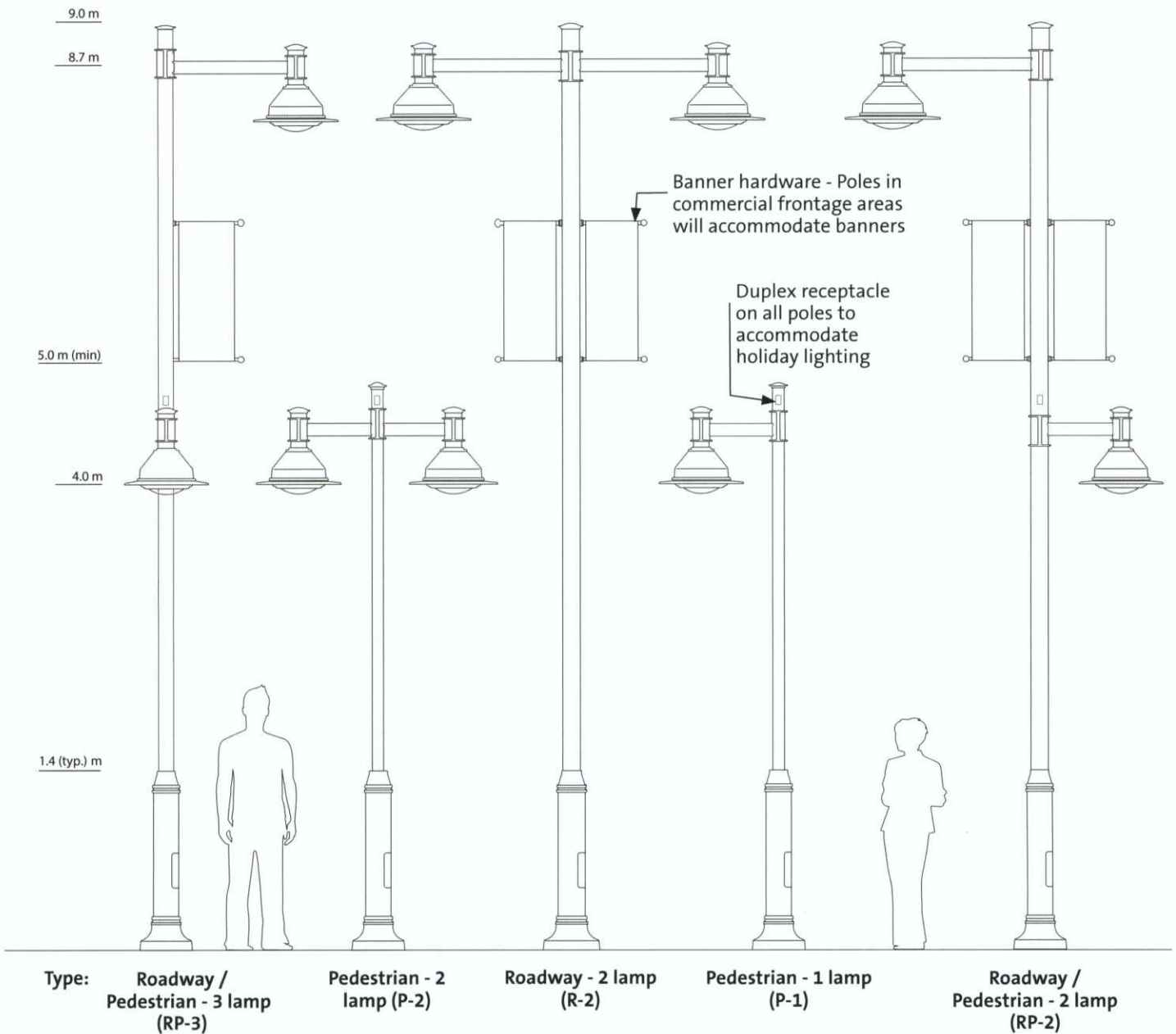
#### Guidelines:

Some example configurations are provided in **Figure 12**. Developers will optimize the placement, model, and spacing within the current requirements to ensure uniformity, safety, and universal accessibility is achieved for all users.

### 4.3.4 Node and Plaza Lighting

Safe and comfortable lighting will be provided for **Primary, Major and Minor Nodes**. Lighting levels are to meet with industry standards, BC Building Code and the City of Coquitlam requirements. Pedestrian level lighting rather than roadway level lighting fixtures should be used. An exterior rated GFCI plug to enable seasonal lighting installations should be provided at a minimum elevation of 2.4m.

Figure 12: Example configurations of approved Roadway & Pedestrian lighting options



**Manufacturer:** Philips Lumec  
**Transit series**  
**Luminaire:** TR-20  
**Mount:** SN  
**Colour:** Semi-gloss textured black





### 4.3.5 Lighting on Non-Designated Streets

Decorative lighting is currently applied to high and medium density streets not designated by streetscape levels. Streets within the BLNP area not designated with a streetscape level will apply standards set in the City of Coquitlam's *Subdivision and Development Servicing Bylaw No. 3558, 2003*.

## 4.4 LANDSCAPING

Landscaping shall follow the city's "Approved Street Tree List" and shall comply with guidelines established by the City of Coquitlam's Urban Forestry.

### Greenway and Node Plantings

Plantings in **nodes** should enhance the sense of place within Burquitlam – Lougheed as **gateways** to the neighbourhood, particularly for pedestrians and cyclists traveling along **Citywide Greenways**. A limited palette of plants with high aesthetic value and easy identification that are present throughout the neighbourhood should be used.

### Street Trees

The appropriate street tree selection is critical to developing the character of the area and selecting species that are long lived and have minimal impact on the surrounding public realm elements allows for reduced maintenance costs. Street tree plantings should seek to establish a continuous tree canopy along primary street corridors of character areas. Appropriate tree selection will be guided by the *Tree Management Bylaw No. 4091, 2010* in conjunction with the City of Coquitlam's Urban Forestry Technician. Tree and underground utility placement must be coordinated to adequately accommodate both and avoid conflict.

### Building Frontage Zone

Plantings in commercial frontage areas should, if there are no plantings on the development parcel, flow into the **Building Frontage Zone** as an extension of the **Furnishing** or **Boulevard Zone** plantings to create a strong sense of continuous public realm from curb to building face. In residential frontage areas, or areas of lower density residential, development parcel plantings should come up to the back of the sidewalk where there are no utility easement restrictions. Planting will be guided by the City of Coquitlam's preferred tree and planting lists.

### Boulevard / Furnishing Zone Plantings

Select appropriate plantings for the **Furnishing Zone** and **Boulevard Zone** to add seasonal interest, biodiversity, colour, green character and environmental benefit. They will also help create a sense of place. The plans suggest critical areas where plantings should occur but also indicate that there are opportunities for developers and landowners to "adopt" portions of the **Boulevard Zone** for additional beautification with planting areas.

*North Road was constructed in the mid nineteenth century by the British Royal Engineers to connect New Westminster to the year-round port facilities in Port Moody.*

## 4.5 PUBLIC ART

Public Art is encouraged throughout the BLNP and is required at **Gateways** and some **Nodes**.

For general guidance on Public Art, please refer to Part 4 of the *Citywide OCP: Urban Design + Development Permit Areas* and the city of Coquitlam's *Public Art Policy and Project Plan* (2011).

In all cases, public art that is not incorporated into the pavement itself should be located outside the **Movement Zone** of all sidewalks and be either within the **Furnishings Zone**, where space is adequate such as at sidewalk bulges, or within the **Building Frontage Zone**, where it will not conflict with pedestrian movement to building entries or other desire lines.

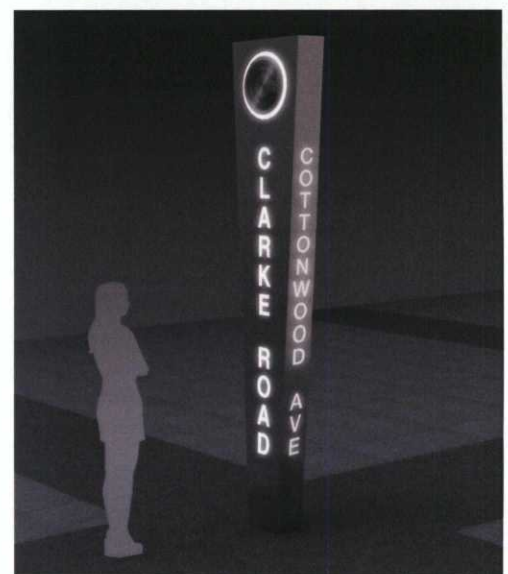
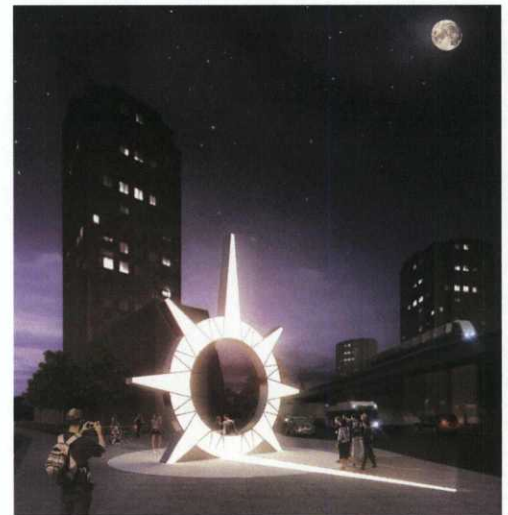
Public safety is a universal criteria for all types of public art; art should not have sharp, protruding, or overhanging edges that can trip or catch a passer-by. In addition, features and associated lighting will be reviewed to ensure the features and associated lighting are not a distraction to road and sidewalk users.

Public art will be reviewed by municipal staff, with the advice of professional artists from the community to ensure that the art meets the criteria in the guidelines and is of artistic excellence and high quality.

**Gateways**, plazas, and **nodes** help commemorate a special place or event. Along with parks and greenways, they are excellent locations for public art, and gathering places in their own right. Please refer to *BLNP Schedule F: Districts* and *Schedule E: Urban Design Framework* to identify these locations.



*Example Gateway Feature South End Median (south of Gatineau). The concept shows an elevated star structure in the road median. It features changeable accent lighting.*



*Conceptual Examples of Public Art as proposed for North Road*



Conceptual artwork for bench medallions to be used in Streetscape Levels 1a & 1b, which recognize the significance of North Road and Clarke Road



## 4.6 WAYFINDING

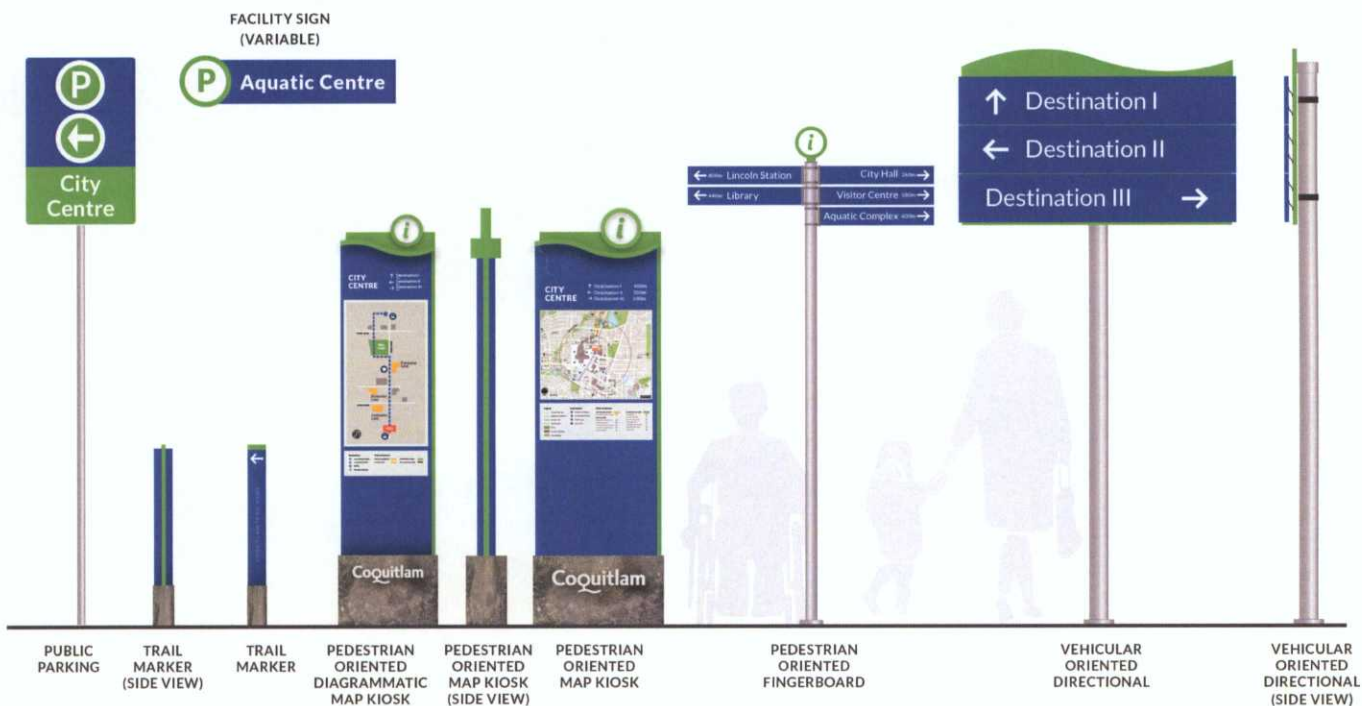
Wayfinding systems connect residents and users to the City’s major facilities and projects, and allow individuals to easily enter a neighbourhood and easily navigate through it.

All **Primary** and **Major nodes** shall be provided with wayfinding as per the *Citywide Wayfinding Strategy*.

In addition, the City of Coquitlam has developed corridor themes for the Burquitlam-Lougheed Neighbourhood, which focus heavily on the strong history of this neighbourhood. Given North Road’s prominent place in history as the second oldest road in the lower mainland, running due North and connecting New Westminster with Port Moody, the North Road theme is heavily tied to the North Star, compass imagery and the concept of multimodal transportation.

**Figure 13** shows examples of the City of Coquitlam’s selection of potential North-Road and Burquitlam-Lougheed Wayfinding Tools and Signage for reference.

Figure 13: Examples of Wayfinding Signage & Street Markers





## 5.0 IMPLEMENTATION

Over the next 20 to 25 years the new Burquitlam-Lougheed Neighbourhood Plan will guide growth and reinvestment in the neighbourhood to help create a complete community. This companion document is intended to ensure that as that growth occurs, and as the streetscapes in this neighbourhood evolve, they will be continually improved and will be guided by the core design objectives within this document.



*Some examples of existing Node and Plaza spaces*



*A Residential streetscape showing a planted boulevard*





# Coquitlam

# For Council

February 8, 2015

Our File: 16-8650-20/SYSW1/2014-1

Doc #: 1869899.v2

To: City Manager

From: General Manager Strategic Initiatives

Subject: **Evergreen Line Corridor - City Enhancement Plan Update**

For: **Council**

**Recommendation:**

That Council:

1. Support the North Road corridor enhancement plan as developed to date in coordination with the City of Burnaby, for further refinement and report back to Council;
2. Support the Clarke Road corridor enhancement plan as developed to date for further refinement and report back to Council; and
3. Endorse the Final Accent Lighting Plan as proposed for Pinetree Way (see Attachment C) in order to enable project tendering at this time.

**Report Purpose:**

The purpose of this report is to update Council and seek approval for the Evergreen Line corridor enhancement plans for North Road, Clarke Road and Pinetree Way.

**Strategic Goal:**

The proposed enhancement projects support the City's strategic goals of enhancing sustainability of City services and infrastructure, and enhancing the transportation system.

**Background:**

Council has received numerous reports and updates regarding proposed Evergreen Line (EGL) corridor enhancements, including a focus in 2014 on completing the detailed design of the Pinetree Way corridor slated for tendering and construction in February 2015 in order to coordinate with the EGL construction schedule. The travel lanes, median and a number of streetscape improvements for Clarke Road and North Road were previously considered by Council and negotiated into the works to be completed by the Provincial EGL contractor with cost sharing from the Province, TransLink; and for North Road median works the City of Burnaby. This approach has enabled the opportunity to minimize disruption to the public and businesses.

In addition to the corridor improvements, public art will be incorporated into the Evergreen Line Stations in Coquitlam. The art is to be funded by the Province with Parks & Recreation & Culture staff and the City's own Public Art Task Force coordinating the actual selection and ultimate implementation of the art pieces.

While the focus of the Evergreen Line corridor enhancements has been on the corridor, staff also believes that there is a need to look at connectivity to and across the corridor in support of the Strategic Transportation Plan's multi-modal objectives. The Burquitlam-Lougheed Neighbourhood Plan (BLNP) update process is currently underway at this time with the City Centre Neighbourhood plan on the horizon thereafter. Both of these plans have the opportunity to build on the corridor planning work that is occurring now and to look for ways to continue to advance the multi-modal nature of the area and to build on that desired sense of community and place making.

**Discussion:**

While many corridor initiatives are already underway, this report provides Council with an update and opportunity for input with respect to specific continued planning efforts along the North Road and Clarke Road corridors, as well as the final accent lighting plan for Pinetree Way prior to project tendering. Enclosed as Attachment A are slides from the proposed Council presentation which summarize key proposed project elements.

It is important to note that to varying degrees along the North Road and Clarke Road corridors, achievement of the ultimate corridor vision is subject to refinement through future Neighbourhood Planning processes as well as reliant on redevelopment of the area. Property takings, road dedications and frontage improvements are an integral obligation of development and are necessary to achieve full streetscape improvements in this area. Unlike along Pinetree Way where the City already has sufficient right of way in place to complete most improvements, on Clarke Road and North Road the City needs to be selective in what can be achieved for 2016. It is also noted that achievement of the ultimate corridor visions is also reliant on good connectivity between the City and private public realms in order to achieve a cohesive, more walkable and livable complete product for the community as redevelopment proceeds.

Pinetree Way

Council will recall the most recent background with respect to the Pinetree Way Improvement Project in 2014, which included approval of the geometric and urban design concept, as well as approval of the light columns, street markers and other decorative features. Also in September, 2014, the City hosted a public information session of which approximately 50 people attended. Although only a few written comments were received, the general feedback indicated support for the project (see Attachment B).



The Pinetree Way works need to be constructed between Spring 2015 and Summer 2016 in coordination with Evergreen Line construction so that they can be in place for opening of the ALRT Line. Accordingly, tendering of the majority of works is scheduled for February in order to achieve this time line. Prior to tendering, staff is seeking Council concurrence with the final accent lighting plan for the guideway, the linear plaza area south of Guildford Way and The Pines feature area south of Lincoln Avenue at this time. The proposed accent lighting concepts are summarized in Attachment C in tabular form and illustrated in Attachment A, Slides 3-7. The accent lighting is intended to provide interest and highlight key features in the heart of the City Centre at intervals along the Pinetree Way corridor.

#### North Road & Clarke Road

The public expressed good support for the conceptually proposed improvements on Clarke Road and North Road a number of years ago during early consultation with the community during the development of the Transit-oriented Development Strategy. Since that time, there has been further definition and refinement of the softer landscape and accent lighting elements of the plan. Accordingly, the updated corridor concepts have been made available on the City's website and displays boards were also included at the first two Burquitlam-Lougheed Neighbourhood Plan (BLNP) public meetings, which were very well attended. Input from these meetings is still being compiled at this time; however, the improvements were generally well received, and there were a number of helpful comments and insights provided by the public.

#### North Road

North Road is jointly owned and operated by Coquitlam and Burnaby. An outdated corridor operating agreement between the Cities is currently under discussion between respective staff. It is intended that this operating agreement be updated to better reflect current operating needs and protocols as well as to recognize any additional infrastructure within the roadway inclusive of the EGL.

Council previously approved an improvement plan for North Road which included a cost shared approach to achieving median improvements along North Road with Burnaby and the Province as illustrated on Slides 20-21 of Attachment A. The focus of improvements is on median construction, enhanced planting, median trellis in select locations and guideway accent lighting. A few curb bulges have also been constructed along the northbound side of the road in order to shorten pedestrian crossing distances, increase intersection efficiency, provide a planting opportunity, as well as to support the introduction of bus/short term parking opportunities along the corridor (see Slide 19, Attachment A). Due to space limitations at this time, it is not possible to widen the sidewalk and boulevard area to accommodate additional walk/cycle area, tree planting, underground hydro,

or other street furnishings. Instead, these improvements will need to be considered in conjunction with the current Neighbourhood Plan update and as redevelopment opportunities along the corridor occur in the future. Due to the relatively shallow depth of properties between North Road and Whiting Way to the east, there also needs to be particular care taken to balance viable development site needs of these adjoining lands with those of achieving ultimate streetscape objectives. Currently Whiting Way is the City's prescribed on street cycle route parallel to the EGL in the area, with opportunity for adjustment through the Neighbourhood Plan update process.

The original concept plan for enhancements along the Coquitlam side of North Road has now been further developed with assistance from ISL Engineering and EOS Lighting consultants as presented in Attachment A, Slides 11-23. The various components of this concept plan are also summarized in table form in Attachment D.

As noted, the majority of planned works for North Road are located in the median and are subject to concurrence and joint funding with Burnaby. Burnaby staff also need to go through an internal review and approval process with respect to the more detailed concepts advanced here, which is underway at this time.

#### Clarke Road

As earlier referenced, many road improvements including turning lanes, medians, sidewalk and multi-use pathway improvements are already underway or completed along Clarke Road (Attachment A, Slides 30, 31, 37, 38). As with North Road, some of the finer aesthetic corridor improvement concepts have been further developed and refined and are presented in both the Attachment A presentation, Slides 24-41, as well as summarized in table form in Attachment E.

It is noted that there are many areas along Clarke Road that require redevelopment to occur in order to achieve ultimate road design features. The west side of the road between Smith and Como Lake Avenue for example is reliant on redevelopment to occur in order to achieve streetscape improvements. With Bosa/Safeway proceeding with their development on the southeast corner of Como Lake/Clarke, frontage improvements will be completed in conjunction with that development at the appropriate time during build-out. The Province is completing the Burquitlam Plaza area; however, staff has identified an opportunity to link the station plaza area to Smith and to the start of the Bosa/Safeway frontage in order to achieve a more finished appearance and to better accommodate anticipated pedestrian loads in the immediate station area. Accordingly, staff is working with all parties including the owner of the Burquitlam Plaza in order to obtain a right of way for a wider sidewalk and a furnishings zone to achieve



continuity there. Council will also recall that there was a transit integration plan commissioned jointly by the City and TransLink to look at how best to integrate transit movements into the emerging urban fabric of the Burquitlam Station area in the longer term (illustrated in Attachment A, Slide 29) which was referred to the Neighbourhood planning process for further consideration. This concept, along with possible adjoining plaza concepts, is contingent on neighbouring lands redeveloping and will not be in place for opening day of the EGL.

The Province has indicated a willingness to provide a multi-use pathway right of way to the City to accommodate the proposed path, plantings and lighting north of Como Lake to near Kemsley (Attachment A, Slides 37-39). Staff is currently also exploring the possibility of creating a nodal public/community space on the north-west corner of the Como Lake & Clarke intersection with a joint City/Neighbourhood Association community sign in this area. The size and extent of this space is subject to the Province granting some form of right of access and use across this land, and possibly additional residual south tunnel portal lands.

**Financial Implications:**

The approximate cost of the North Road/Clarke Road Works is on the order of \$10 million inclusive of Provincial, TransLink and City of Burnaby contributions, with \$7.6 million of this being City of Coquitlam budgeted works. The Pinetree Way Enhancements are still estimated at \$14 Million. Coquitlam's portion of funding has been included in the 5-year capital plan in a phased manner over a number of years with the final North Road/Clarke Road funding of \$1.1 million shown to be funded in 2016.

Public Works and Parks Operations have been involved in the process throughout concept development and design in order to closely manage operating costs. It has been initially estimated that operating costs for the Pinetree Way works will increase on the order of \$180,000, with estimates for North Road and Clarke Road still to be completed based on the refined concepts presented.

**Conclusion:**

The EGL corridor improvement plans are now well developed with further refinements planned for the North Road/Clarke Road corridors following Council and public input. The final accent lighting details for Pinetree Way are presented for approval in order to enable tendering of this work in February and a spring construction start in coordination with the Evergreen ALRT Line construction.

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**Attachments:**

- Attachment A: Presentation Slides Summarizing Key Project Elements
- Attachment B: Sept. 2014 Pinetree Way Information Session Public Feedback
- Attachment C: Pinetree Way Accent Lighting Features Tabular Summary
- Attachment D: North Road Corridor Enhancements Tabular Summary
- Attachment E: Clarke Road Corridor Enhancements Tabular Summary

This report was prepared by Ian Radnidge, Director Strategic Projects and Mark Zaborniak, Manager Design and Construction.



**Pinetree Way Enhancement Project Comment Feedback Forms from Info Session on Sept. 17<sup>th</sup>, 2014**

Comment Form No.	Did the information presented give you a complete understanding of the project?	What were we missing?	Do you have any other comments or feedback?
1	Yes	Everything was good	Yes, put the light columns in the middle of Coquitlam like video games! Example, like the main plaza spawn town in games! (A huge village in middle of Coquitlam). If you do this YOU ROCK!! Or replace the light columns with a huge fountain!
2	Yes, very good, video was excellent	Nothing I was aware of	The end product looks to be awesome. So good that I think Richmond, Surrey, Burnaby and Vancouver could learn a thing or two.
3	Yes	Directional arrows on the signs/posters	the seating areas in 'The Pines' need to be discouraging to late night / overnight usage. No need to attract loitering and gathering around the SkyTrain station area.
4	Yes, quite detailed and good information. You have done your homework. Great job so far.	Nothing that you have missed.	Looking forward to completion and the new SkyTrain accessibility for our citizens including Douglas College students and access to the Evergreen Art & Cultural Center and City Hall and City Centre Park. Staff on hand very knowledgeable and answered all my questions.
5	Yes, nice project.	(blank)	Make Coquitlam more active, more bright (lighting) and energetic City :).
6	Somewhat.	More details re: signage, lighting, etc.	Brought up safety issues re: switching bicycle lanes and pedestrians. Also, column lighting & floor lighting opportunities.

**Pinetree Way Final Accent Lighting Plan**

	Enhancement Feature	Description
1	The Linear Plaza	This feature is located in a very prominent location along the multi-use pathway, leading up to the Lafarge Lake-Douglas Station on the east side of Pinetree Way, south of Guildford Way. The pathway surface will have various in-lay paving treatments and colors, which will provide the appearance of a meandering stream. Also, accent lighting will be projected from seating benches located under the guideway in this area in order to create the appearance of a moving stream on the underside of the guideway. People using the plaza will be able to interact with the lighting display through movement of their hands over the projections.
2	The Pines	This feature is located on the west side of Pinetree Way south of Lincoln Avenue, along the multi-use pathway taking advantage of a location where there is adequate space between the roadway and the guideway to provide a significant area for a landscape feature. The feature lighting will be mounted at the base of the planting beds/seating, to provide interest and to highlight the landscaping.
3	The Guideway (Barnet- Guildford Way)	The accent lighting concept for Pinetree Way guideway Lighting is to have column lighting activate as the ALRT approaches and departs the stations. The columns will light up sequentially with ALRT car movements; at this time lighting is blue in the northbound direction and green in the southbound direction (similar colors to Coquitlam’s logo)(colours are fully programmable and may need to change if they create visual conflicts for drivers). The columns will then sequentially fade out as ALRT train moves away. At the library, the guideway crosses over Pinetree. This was identified as an opportunity for City messaging / creative content. Accordingly it is proposed that projections be made onto the south face of the guideway crossing in this location. As illustrated on enclosed slide images, the City can have slide projection images pre-manufactured to project on the guideway. TransLink is currently reviewing what content is acceptable to project onto the guideway.



4	Other Accent Lighting (Approved July 28/14)	As noted in the July 2014 report, the light columns will be the most dramatic features in this project – and therefore will be the most brightly lit - noting that for energy efficiency, LED lights will be used. As noted in the previous report, the light columns will change color in response to street activity, including the ALRT. The “Branches” were also discussed in the July report, and the intent is to include accent lighting so that this feature provides interest both in the daytime and at night .
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## North Road Corridor Enhancements

	Enhancement	Description
	<b>For 2016</b>	
1	Concrete median enhancements	Rain water infiltration, enhanced landscaping (c/w irrigation), trellis in select locations
2	Planted curb bulges at Cochrane, Cameron & Foster	Provide opportunity for tree planting where limited opportunities exist on the corridor, introduce parking/bus stop lanes, shorten pedestrian crossing distance of North Road, improve intersection efficiency.
3	Guideway accent Lighting – possibly phased	Concept/theme of navigation, North Star, associated with north-south alignment of North Road and of its history as a goods & people movement corridor
4	Possible street markers/wayfinding -phased	North Road obelisks, or marker posts and/or other wayfinding signs are all under consideration for the North Road corridor at this time (to include corridor theme)
5	Street Banners on existing light standards where possible	Many poles along North Rd. already have banner brackets; it is proposed to enhance the level of banner coverage on the corridor. Staff are also investigating the feasibility of painting the corridor poles black as an interim measure before full streetscape enhancements occur through redevelopment
6	Gateway features	Proposed near Gatineau in the median and at intersection of North Rd & Clarke Rd.. Austin is also a significant Gateway to Coquitlam which will be considered further through the BLNP.
	<b>Longer term through redevelopment</b>	
1	Nodal plaza/enhanced gateway areas	There is an opportunity to consider nodal enhancement/plaza areas along the corridor through the BLNP process (provide place making, public refuge & opportunity to pause). The concept for a plaza near Gatineau would provide an important continuity/linkage to the Lougheed Station Plaza area. It could also provide opportunity to recognize the diverse cultures of Coquitlam. The north end gateway feature and plaza area (intersection of North Rd and Clarke Rd) could be expanded/completed with the provision of additional land through redevelopment
2	Boulevard & Sidewalk enhancements	Enhanced lighting including pedestrian scale, wider sidewalk and planted street

		furnishings zones. Possible undergrounding of hydro lines along the corridor. These are not possible at this time due to insufficient road corridor width and existing buildings and property. As redevelopment occurs, these works can be completed by developers.
3	Burnaby	Coquitlam is working closely with Burnaby and the Province to achieve North Road median enhancements and accent lighting. A number of improvements have already been completed on the west side of North Rd. by the Province in order to widen the road for the guideway. In addition, with the anticipated redevelopment of Lougheed Mall significant additional enhancements are possible. Burnaby also proposes to realign the intersection of North Rd. (north segment) where it intersects Clarke Rd.



## Clarke Road Corridor Enhancements

	Enhancement	Description
	<b>For 2016</b>	
1	Turning lane improvements at Como Lake and Smith Ave.	As highlighted on enclosed plans, additional turning lanes are in the process of being added to better accommodate traffic movement in the area. Laning and boulevard improvements are not possible south of Como Lake on west side of Clarke until redevelopment occurs as the works would intrude onto private property. A new signalized intersection will be located at the Station/Mall entrance which is also aligned with a future east-west roadway. With the Beedie Living project it is proposed that this lane be one-way in the east-bound direction on the west side of Clarke Rd for the short-term due to property limitations; while the east side will accommodate 2-way movements into the Mall.
2	Province constructing Burquitlam Station plaza area, complete with public art	Includes bus exchange, street furnishings, streetlights, drop off/pick up area, etc. It is also proposed that the City fund Station Name sidewalk mosaics at each of the Coquitlam Stations which is beyond the Provincial funding envelope.
3	Bosa/Safeway construction frontage improvements	Full streetscape frontage improvements are planned by this development. There is a need to incorporate the proposed Clarke Rd gateway feature into this frontage plan, if it is acceptable to Council.
4	City to widen sidewalk & provide planted furnishings zone c/w lighting against the curb on areas of Burquitlam Mall frontage along Clarke Rd beyond the station plaza area	Subject to the Mall providing the City with a right of way, it makes sense to do these improvements at this time in anticipation of the large volume of pedestrians anticipated in the area with a Skytrain Station opening in 2016. If agreement can not be reached, this work will need to be completed by the Mall in the future with redevelopment.
5	Guideway accent Lighting – possibly phased	Concept of evolution of travel and continuation of elements of the north star accent lighting from North Road along Clarke
6	Incorporate wayfinding where possible - phased	Clarke Rd. pedestrian scale signs and wayfinding under consideration

7	Gateway features	Proposed at intersection of North Rd & Clarke Rd. (per North Rd. concept) and S.E. corner of Como & Clarke.
8	Community Message Sign	Work with neighbourhood assoc.'s to achieve a changeable message sign on the N.W. corner of Como & Clarke
9	Multi-use pathway: Como Lake-Kemsley	A multi-use pathway complete with pedestrian scale lights and tree planting is proposed to improve connectivity of local neighbourhoods and enhance aesthetics. The Province has indicated a willingness to provide the City with a right of way for this purpose.
	<b>Longer term through redevelopment</b>	
1	Nodal plaza/enhanced gateway areas	There is an opportunity to consider nodal enhancement/plaza areas at the north and south ends of the proposed multi-use pathway on the south tunnel portal lands subject to the Province making the lands available for such public purposes. Also with redevelopment of the Burquitlam Mall it is possible to create an inviting public plaza with accent lighting with shelter provided from the guideway above (subject to further study/consultation through the BLNP).
2	Boulevard & Sidewalk enhancements	Enhanced landscaped boulevard and widened sidewalks complete with pedestrian scale lighting, tree planting, additional multi-use turn/parking lane for areas beyond where it as able to be completed for 2016 are proposed to be achieved in conjunction with future redevelopment (unfortunately the City's road corridor is not wide enough to achieve these improvements at this time). The wider lands under the guideway between Smith and Cottonwood provide a particular opportunity for further study, possibly some public art, and further consultation through the BLNP process.