



CITY CENTRE STREETSCAPE GUIDELINES

Endorsed: September 6, 2022

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Executive Summary

The *City Centre Streetscape Guidelines* is a companion document to the *City Centre Area Plan* that describes how the streetscapes and public realm will achieve the vision and the community aspirations for the future of City Centre. This vision builds on the concept of creating a compact, complete and vibrant transit-oriented downtown, and emphasizes City Centre as the economic, civic and cultural heart of Coquitlam and the northeast region.

The overarching goal of the *City Centre Streetscape Guidelines* is to support a public realm with design excellence, high quality urban design standards and landscape architecture that speaks to Coquitlam's natural setting and history. Public realm elements should be durable and long-lasting, using high-quality materials and construction standards.

The *City Centre Area Plan* envisions the continuing transformation of Coquitlam's City Centre into a family-friendly and inclusive community as well as a culturally and economically vibrant downtown. The *City Centre Streetscape Guidelines* support this vision in the following ways:

- Design principles and a variety of durable materials will speak to Coquitlam's natural setting of mountains, forests, watercourses and the Coquitlam River.
- High-quality urban design will create a lively and enjoyable public realm that is universally accessible, safe, inclusive, supports a variety of user needs and does not impede the flow of pedestrian movement.
- A micromobility network will enhance active transportation options that provide for safe and convenient walking, biking and rolling infrastructure that reduces conflicts with buses, trucks and automobiles.
- Greenway design guidelines will steer the development and form of City Centre's Citywide and Neighbourhood Greenways to safely support all users.
- Guidelines for site furnishing, paving materials, lighting, landscaping and public art will reflect the city's identity through a modern, unified streetscape with distinct characteristics.
- A Streetscape Enhancement Management Committee of cross-departmental staff will provide comprehensive guidance on an ongoing basis, support City standards, assess procedures and processes for the enhancement of Coquitlam's streets, and resolve issues in a timely manner.

1.0 INTRODUCTION



*Salmon public art sculpture located in Spirit Square references Coquitlam's name which comes from the Coast Salish word *kʷikwə́λəm* (Kwikwetlem) which means "red fish up the river".*

1.1 Intent

The *City Centre Streetscape Guidelines* (CCSG) is a companion document to the *City Centre Area Plan* (CCAP) which was adopted by Coquitlam City Council in November 2020 and replaced the previous 2008 area plan for City Centre. The updated CCAP renews the vision for City Centre as the downtown and heart of Coquitlam and the northeast region by building on the concept of transit-oriented development and recognizing the importance of economic and cultural vibrancy.

The CCSG helps implement the vision and objectives of the CCAP through specific design guidelines for the public realm and streets. The document builds on the design principles and placemaking objectives of the Pinetree Way enhancement project completed in 2016, lessons learned from earlier streetscape guidelines and current best practice construction standards. The CCSG replaces the earlier *Glen Drive Design Guidelines* and *The High Street Design Guidelines*.

A series of street frontage types and Character Streets have been established to support vibrancy and a range of economic, social and recreational activities in City Centre, as identified on Schedule I of the CCAP. The street frontage types and Character Streets identify the role and function of key streets with the goal of creating a safe, vibrant, and pedestrian-friendly public realm. The CCSG provides design guidelines on these streets to enhance and support their use.

Streetscapes and the public realm are significant places in our communities that are intended to be inviting and people-centered destinations. Placemaking concepts support designs that are interesting, inclusive, dynamic and flexible for a variety of uses during different times of the day and year. They help foster a sense of belonging for residents, enable social cohesion by encouraging interaction, and enrich community identity through design that reflects the area's history and natural setting. Most significantly, streetscapes help shape impressions of the public realm as interesting and vibrant places that invite people to come, stay and revisit.

The design of streetscapes and the public realm also contributes to a community's social and economic well-being. Designing with the intention of attracting and inviting people outdoors at all times of the day and year leads to improved physical, social and emotional health outcomes, as well as improved economic vibrancy, as people are more active and engaged within their community.

This document will be used as part of the design and review of development permit applications for properties along a street with a designated street frontage type or Character Street, as identified on Schedule I of the CCAP and **Figure 5** of this document. This area generally falls within the Downtown Core where majority of the high density and mixed-use development is anticipated in City Centre.

This document will also be used for guidelines on micromobility, greenways and the Linear Park as shown on Schedule E and Schedule G of the CCAP, and **Figures 11** and **12** of this document. Guidelines are also provided on street furniture, paving materials, decorative lighting, landscaping, public art and wayfinding.

Streets without a designated street frontage type or Character Street will be guided by the standards set in the *Subdivision and Development Servicing Bylaw*, Part 4 (Urban Design and Development Permit Areas) of the *Citywide Official Community Plan (CWOCP)* and other City standards and requirements.



Public realm enhancements including landscaping, lighting, unique street furniture and paving treatment under the Evergreen Line SkyTrain Guideway along the Pinetree Way Character Street and Citywide Greenway.

1.2 Guiding Principles

Achieving high-quality urban design and functional streetscapes will be essential to the success of the CCAP. As visitors and residents move between major destinations, commercial hubs and residential areas, it is vital that the public realm and streets maximize the safety and comfort for all users. The design of the public realm will support an enjoyable experience, enhance the area's vibrancy and enable a convenient environment for pedestrians and micromobility users.

The CCSG aim to achieve these goals by focusing on three Guiding Principles:

Prioritize universal accessibility, public safety, a pedestrian-friendly environment and active modes of transportation.



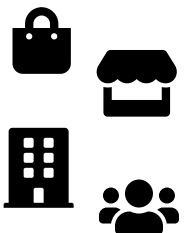
- Streets will be designed for the safety and comfort of all ages and abilities, and for various active modes of transportation. Streets will support vulnerable groups such as children, women, seniors and people living with disabilities by ensuring a usable, safe and easy to navigate public realm, at all times of the day and year.

Enhance natural areas and access to nature, and demonstrate environmental sustainability.



- Trees, landscaping and green infrastructure elements will be integrated in the design of the public realm to enhance access to green space, reduce urban heat island effects, and improve stormwater management and biodiversity through the streetscape.

Demonstrate urban design excellence to support businesses and economic development opportunities and to enhance the area's civic and cultural heart.



- The public realm will demonstrate urban design excellence and be designed as unique, attractive and multi-functional destinations that draw people in and encourage gathering. This will in turn support local businesses, attract future investment and contribute to the overall success of City Centre, and the three precincts centered around SkyTrain stations and serving as welcoming destinations along the Pinetree Way Character Street.

In addition to the Guiding Principles, the CCSG will support the vision and principles of the CCAP, in the following ways:

- Sidewalks, boulevards and a micromobility network will enhance the walking, cycling and rolling experience, and support the three transit-oriented precincts that will function as destinations and activity hubs around SkyTrain stations.
- Streets will have landscaping and integrate green infrastructure to improve biodiversity, support stormwater management and enhance equitable access to green space.
- The public realm will be designed to provide visual interest, foster a sense of place and enhance unique destinations in City Centre.
- Streets will have appropriate surface treatment to support the various needs of residential and commercial frontages.

1.3 How to Use the Streetscape Guidelines

The CCSG is to be used in the design and review of works and services known as frontage improvements, and for streetscape and other public realm improvements on streets with a designated street frontage type or Character Street, as identified on Schedule I of the CCAP and **Figure 5** of this document. These improvements will typically be completed as part of new development and coordinated with other projects in City Centre.

The guidelines in this document address the design of the following streetscape elements:

- Building Frontage Zone
- Pedestrian Movement Zone
- Furnishing Zone
- Boulevard Zone
- Micromobility Zone
- Buffer Zone
- Micromobility
- Greenways
- Linear Park
- Street furniture
- Paving materials and treatment
- Decorative lighting
- Landscaping
- Public art
- Wayfinding

The CCSG supplements street design standards in the *Subdivision and Development Servicing Bylaw*, Part 4 (Urban Design and Development Permit Areas) of the *Citywide Official Community Plan (CWOCP)*, and other City standards and requirements.

When beginning a streetscape design or review of a development permit application, the progression outlined below should be followed. The listed Schedules refer to those in the CCAP, and the figures and sections refer to those in the CCSG.

Step 1: Refer to **Figure 5** or Schedule I to determine whether the subject property is along a designated street frontage type or Character Street.

Step 2: Review **Section 1.2 Guiding Principles** to ensure the streetscape and public realm design conforms to the urban design goal of achieving a vibrant, high-quality and universally accessible public realm.

Step 3: Refer to the following sections, figures and Schedules:

- **Section 3.0 Street Frontage Types and Character Streets Guidelines** for guidelines on the design of the public realm.
- **Section 4.0 Micromobility, Greenways and Linear Park** for guidelines on each infrastructure.
 - **Micromobility:** Refer to **Figure 11** to determine if the subject property is part of the micromobility network.
 - **Greenways:** Refer to **Figure 11** or Schedule G to determine if the subject property is along a Citywide or Neighbourhood Greenway.
 - **Linear Park:** Refer to **Figure 11** and **12** or Schedules E and G to determine if the subject property is along the Linear Park.
- **Section 5.0 Street Cross-Sections** and **Appendix A** for detailed cross-sections which will guide road dedication requirements.

Step 4: Refer to **Section 6.0 Public Realm Furnishings** for guidelines on street furniture, paving materials, decorative lighting, landscaping, public art and wayfinding.

Applicants are encouraged to pursue additional unique, innovative and high-quality streetscape designs to complement the guidelines outlined in this document. In some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager of Planning & Development.

Should a conflict exist between the application of the streetscape design guidelines identified in this document and the typical City of Coquitlam standards and requirements, these guidelines take precedence subject to staff discretion.

2.0 STREETSCAPE ELEMENTS



A variety of street furniture and landscaping on the Pinetree Way Character Street and Citywide Greenway.

2.1 Definition of Streetscape Zones

The CCSG defines the streetscape as the area from the building face to the curb. The streetscape consists of five zones that serve different functions, which include the **Building Frontage Zone**, **Pedestrian Movement Zone**, **Furnishing Zone**, **Boulevard Zone**, **Micromobility Zone**, and **Buffer Zone**.

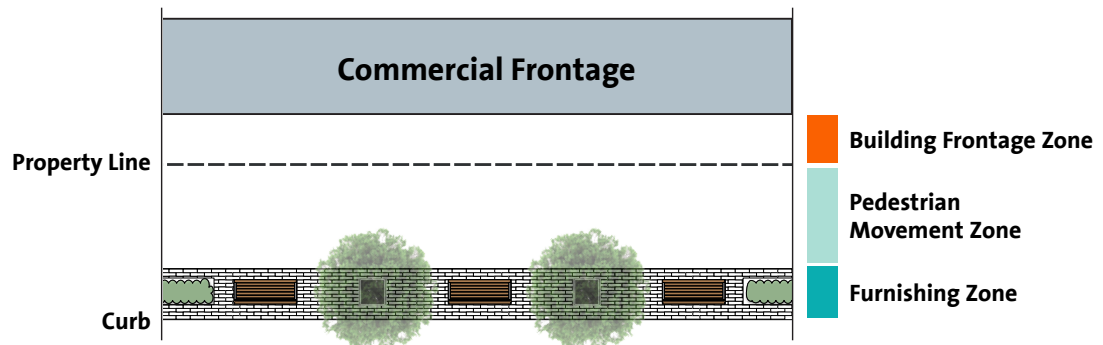


Figure 1 - Streetscape Zones Along Commercial Frontages.

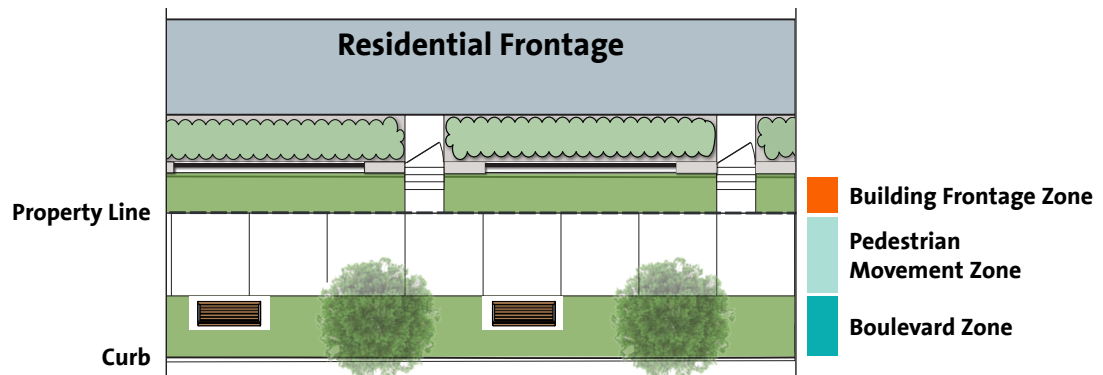


Figure 2 - Streetscape Zones Along Residential Frontages.

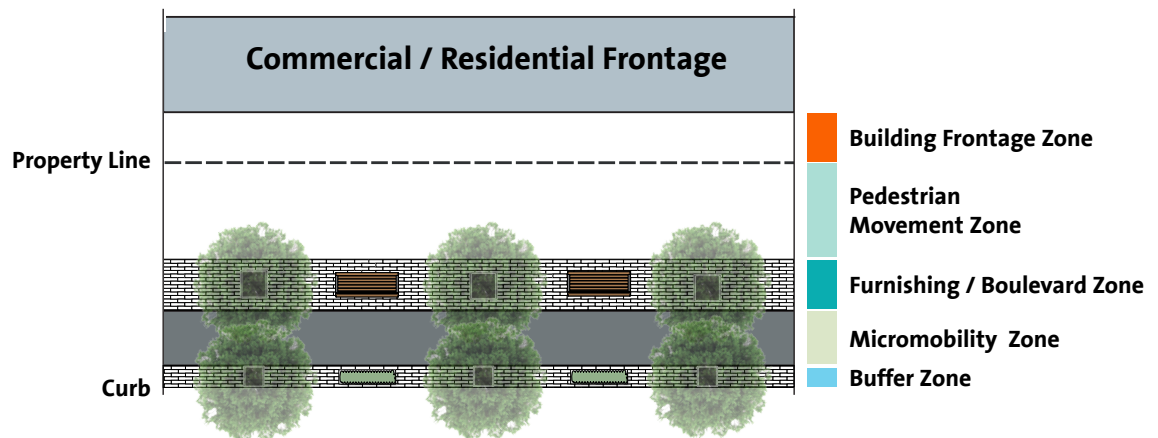


Figure 3 - Streetscape Zones Along Micromobility Network.

Note:

1. Drawings not to scale. Provided for illustrative purposes only.

Building Frontage Zone

The **Building Frontage Zone** is the area between the building face and the **Pedestrian Movement Zone**. To enable an active and vibrant street frontage, a variety of elements, such as seating, landscaping, planter boxes, merchandise displays, and cafe and restaurant seating may be sought within this zone. Vibrancy can be further enhanced where commercial uses such as cafes open onto the street with garage door-style access or other building design elements that introduce openness and transparency between indoor and outdoor spaces.

Pedestrian Movement Zone

The **Pedestrian Movement Zone** or “sidewalk” is the area intended for pedestrian travel for people of all ages and abilities. This zone is free of obstacles, continuous, uniform and predictably located between the **Building Frontage Zone** and the **Furnishing / Boulevard Zone**. The **Pedestrian Movement Zone** will support universal accessibility and accommodate people with vision impairments, mobility challenges, including the use of wheelchairs, strollers and other mobility aids.

Furnishing Zone

(Applies to commercial fronting streets)

The **Furnishing Zone** is the hard-surface area between the **Pedestrian Movement Zone** and the curb. It buffers pedestrians from the adjacent roadway and is where streetscape amenities, such as seating, bike racks, streetlights, street trees, landscaping, transit shelters and other elements are located, that could be an obstacle to pedestrian movement. Physically separating and buffering vehicle travel lanes increases the comfort of pedestrians and micromobility users. This is especially impactful on streets with heavy vehicle traffic and higher speeds or where street parking does not exist.

Along commercial fronting streets, where landscaping is provided in the **Furnishing Zone**, a hard surface treatment, will be provided next to the curb, to accommodate vehicle door openings, provide a comfortable setback from moving vehicles and street furniture, and offer a barrier-free access to the **Pedestrian Movement Zone**. Where landscaping is provided, sustainability objectives should be supported by providing stormwater management features such as rain gardens.



Spill out space in the Building Frontage Zone to support an active and vibrant public realm.



Uniform and continuous Pedestrian Movement Zone with street furniture, landscaping and street trees in the Furnishing Zone.



Hard surface treatment between the curb and landscaped area in the Furnishing Zone to offer a barrier-free access to the Pedestrian Movement Zone.

Boulevard Zone

(Applies to residential fronting streets)

On residential fronting streets, the area between the **Pedestrian Movement Zone** and the curb is called the **Boulevard Zone**. This area is next to the curb and buffers pedestrians from the roadway. The **Boulevard Zone** is typically landscaped with grass and/or planting and continues to contain the various street elements that would otherwise impede pedestrian movement. The **Boulevard Zone** should be used as an area to support sustainability objectives by incorporating environmentally sensitive designs and stormwater management features such as rain gardens. In some instances, the **Boulevard Zone** can include paving and street furniture such as bike racks or seating.



Soft landscaping and street trees along the Building Frontage Zone and Boulevard Zone on a residential fronting street.

Micromobility Zone

The **Micromobility Zone** applies to streets identified on **Figure 11** and accommodates people using human-powered mobility devices (e.g., bikes, kick scooters) and electric assist mobility devices (e.g., electric bikes, electric kick scooters). The **Micromobility Zone** is either located between the **Furnishing / Boulevard Zone** and the **Buffer Zone** or between the **Pedestrian Movement Zone** and the **Furnishing / Boulevard Zone**.

Buffer Zone

The **Buffer Zone** is located along streets where micromobility facilities are required and located between the curb and the **Micromobility Zone**. The zone provides a buffer between micromobility users and moving or parked vehicles. Elements such as utilities and parking metres can be located in this zone. This space can be landscaped, paved or a combination of the two.



Micromobility Zone located between the Furnishing Zone and Buffer Zone with a combination of pavers and street trees.

2.2 Other Streetscape Elements

Driveway Letdown

Lanes and on-site parking will be designed with appropriate driveway letdowns to minimize disturbance to the **Pedestrian Movement Zone** as guided by the standards set in the *Subdivision and Development Servicing Bylaw*. The design will aim to reduce motor vehicle speeds, avoid conflict between automobiles and pedestrians, and prioritize pedestrian safety by ensuring a continuous **Pedestrian Movement Zone** and universal accessibility for all users.

Lanes

Lanes provide critical access for loading, parking, servicing and emergency access for high-density development. They also offer an alternative means for pedestrian access and opportunities for passive streetscaping and beautification. Where appropriate, lanes are to be enhanced with landscaping, lighting, wrapped retail frontages, and other elements to demonstrate Crime Prevention Through Environmental Design.

Where feasible, lanes should bisect blocks in one or two directions and achieve a finer grid pattern with lane corridors set at approximately 80 to 100 metre intervals (i.e., this is intended to achieve a sub-block vehicular access grid that is up to 200 metres on any one side and 80 to 100 metres on adjoining sides), as noted in Section 6.4 of the CCAP and illustrated in **Figure 4**.

Walkways

Walkways or mid-block walkways are barrier-free and clearly identifiable connections that increase block permeability for active transportation modes and shorten travel distances for pedestrians.

Walkways should consider connections to neighbouring properties and may be located within lane right-of-way allowances or exclusive vehicle-free corridors, and would bisect blocks to achieve a sub-block active transportation grid that is approximately 80 to 100 metres on each side, as illustrated in **Figure 4**.

Refer to Sections 6.3 and 6.4 of the CCAP for further details on Walkways.



Driveway letdown to minimize disturbance to the Pedestrian Movement Zone.



Spirit Square Walkway.

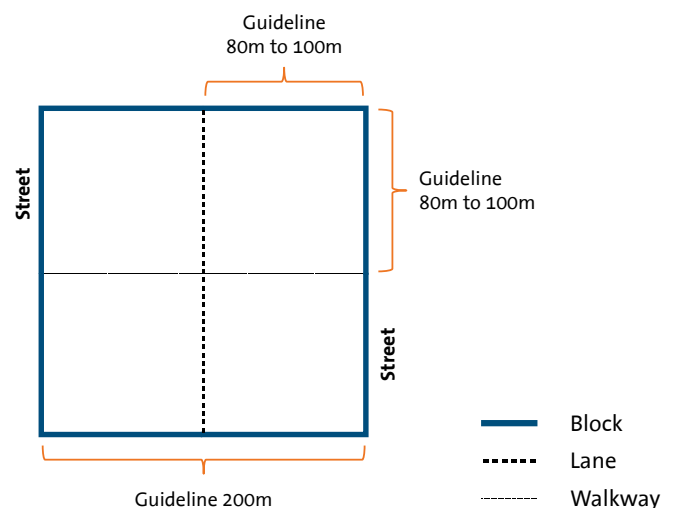


Figure 4 - Illustrative Block Pattern Guideline.
(Lanes and Walkway orientation is not fixed)

3.0 STREET FRONTAGE TYPES AND CHARACTER STREETS GUIDELINES



A clear Pedestrian Movement Zone along a commercial street frontage with landscaping and street trees in the Furnishing Zone.

3.1 Street Frontage Types and Character Streets

The CCAP designates key streets in the Downtown Core as street frontage types and Character Streets to create an animated public realm, enhance vibrancy and support a range of economic, social and recreational activities.

Street frontage types are ground level designations that identify the desired use (e.g., commercial, civic, residential) on streets within the downtown. These street frontage types are complemented by Character Streets possess which key placemaking features to create a unique and distinguishable identity. Character Streets will function not only as a link for mobility and accessibility, but also as an attractive place for people-oriented activities.

To achieve the desired streetscapes in City Centre, the CCSG provides design guidelines as well as detailed midblock cross-sections for streets with a street frontage type or Character Street designation, as illustrated on **Figure 5** and Schedule I of the CCAP. Streets without a street frontage type or Character Street designation will be guided by the standards set in the *Subdivision and Development Servicing Bylaw*.

Street Frontage Types

Mandatory Commercial Street Frontage are street frontages along which development must provide continuous commercial storefront units and/or civic uses.

Secondary Active Street Frontage are street frontages along which development must provide active, ground-oriented commercial, residential, civic and/or employment living uses (e.g., home-based businesses, live/work units).

Residential Street Frontage are street frontages along which development must provide continuous, ground-oriented, residential, civic and/or employment living uses.

Character Streets

Pinetree Way Character Street is a key north-south transportation and commercial corridor in City Centre that connects the Pinetree-Lougheed, Lincoln SkyTrain Station and Four Corners Precincts which are situated around key intersections and SkyTrain stations. Pinetree Way features an attractive streetscape designed with pedestrian-friendly lighting, street furniture, public art, and improved signage and wayfinding elements to encourage people to walk, cycle, use transit and gather in the Downtown Core.

Three gateway features along Pinetree Way will mark the north-south entry into the Downtown Core as well as the entry to the Entertainment District at the Lincoln SkyTrain station, as identified on Schedule J of the CCAP.

“Urban Boulevard” Character Street is located along Lougheed Highway from west of Johnson Street to Westwood Street and is the key east-west arterial corridor that forms part of TransLink’s Major Road Network (MRN) connecting the city to the region. Through redevelopment, this arterial corridor will transform into a walking, cycling and transit friendly boulevard with wide sidewalks, lighting, landscaping, and new development fronting the street with active frontages

Two gateways marking entry into the Downtown Core and one gateway marking entry into City Centre will be incorporated to create distinctive east-west entry points into the Downtown Core, as identified on Schedule J of the CCAP.

“Downtown Promenade” Character Street will be a southern extension of The High Street located in the heart of Coquitlam’s downtown. It will demonstrate urban design excellence and be transformed into a unique, animated and year-round pedestrian-friendly destination with prominent architecture that will support the Entertainment District, Office Business District, and the cultural and civic amenities forming the “Cultural Heart”.

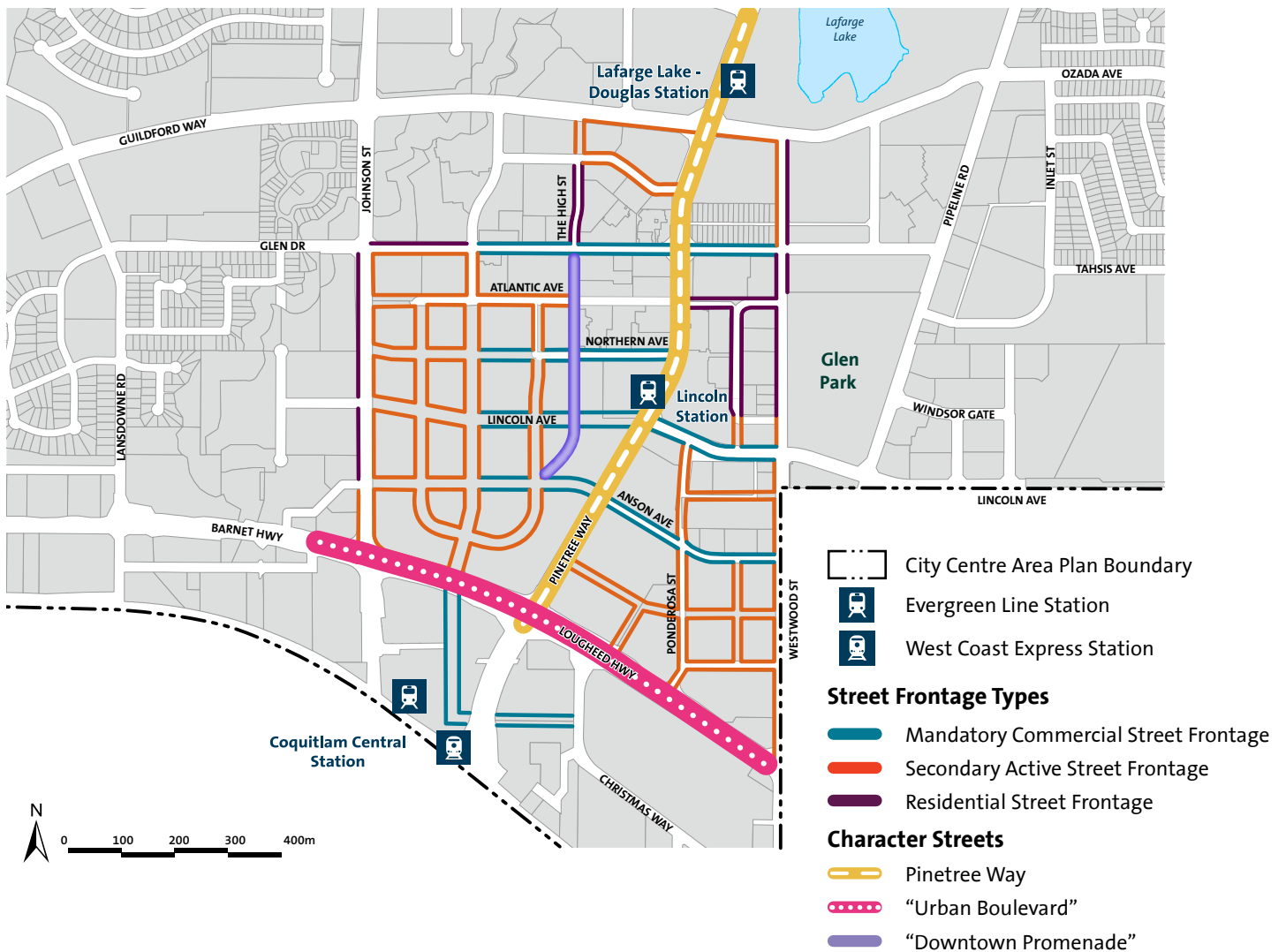


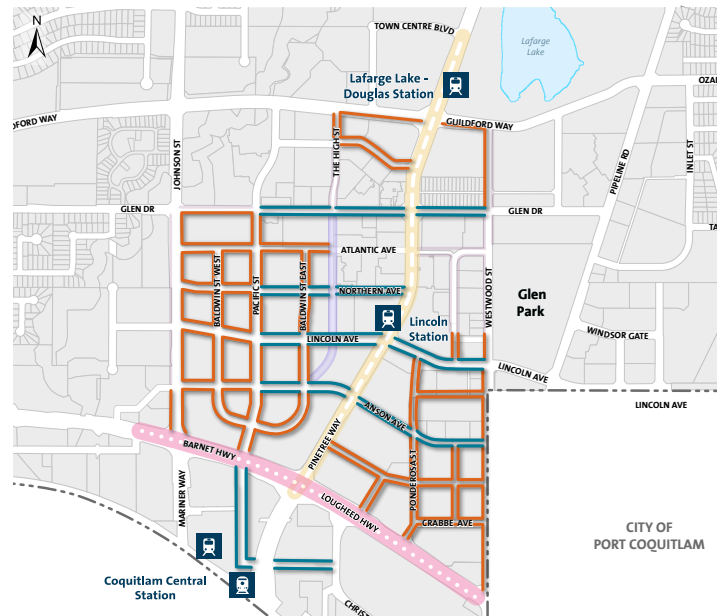
Figure 5 - Street Frontage Types and Character Streets.

Note:

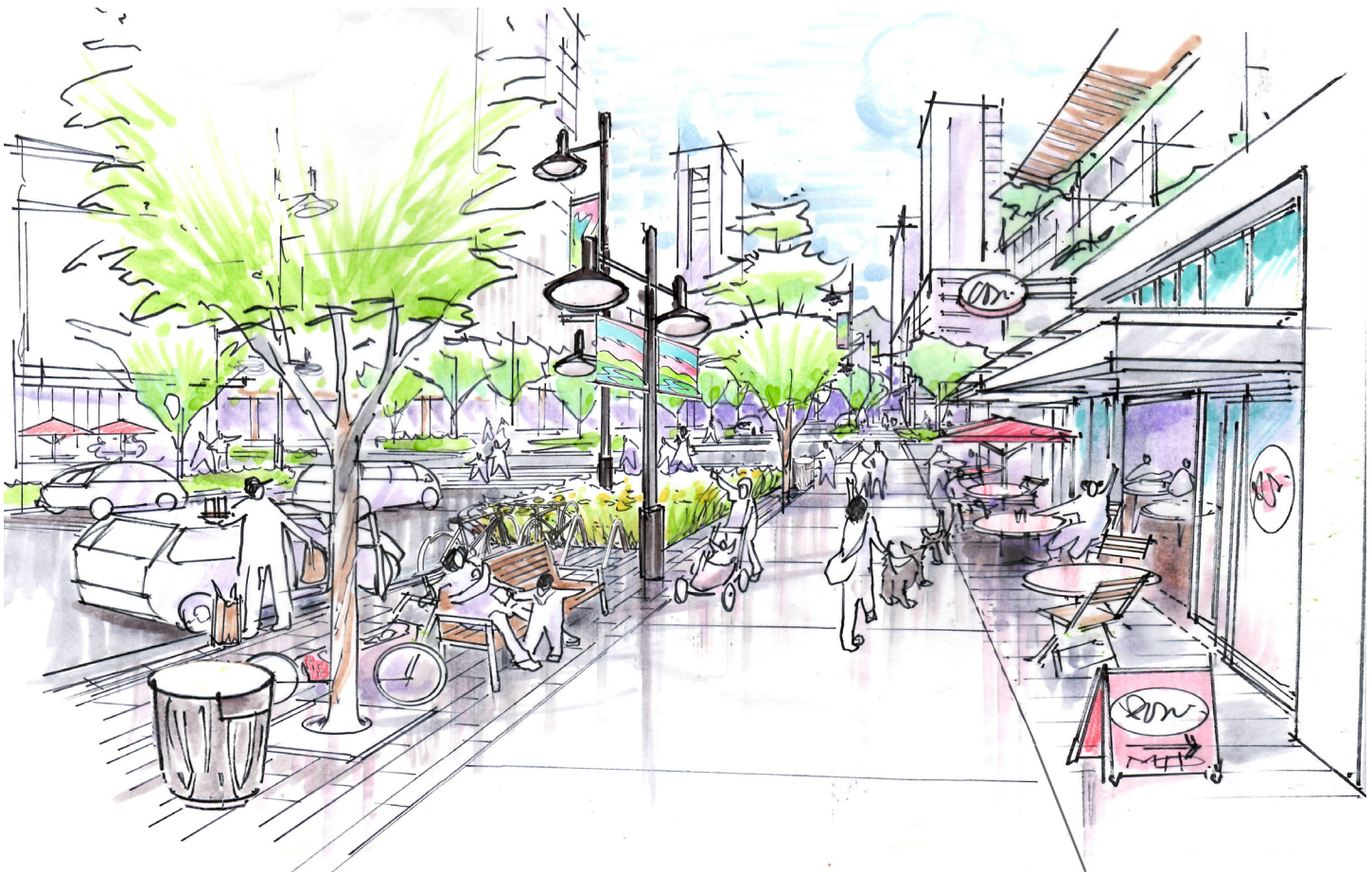
1. Street frontage types and Character Streets may be extended beyond what is shown in order to achieve a consistent streetscape and appearance in key areas of City Centre subject to staff discretion.

3.1.1 Commercial Frontages

Commercial frontages apply to streets designated **Mandatory Commercial Street Frontage** and **Secondary Active Street Frontage**. These streets are characterized by having pavers in the **Furnishing Zones** to support pedestrian movement and activity along ground-oriented uses such as restaurants, cafes, bakeries and retail stores.



Mandatory Commercial Street Frontages and Secondary Active Street Frontages.



*Conceptual sketch of a Mandatory Commercial Street Frontage or Secondary Active Street Frontage.
(Artist's concept - It will not be built exactly as shown).*

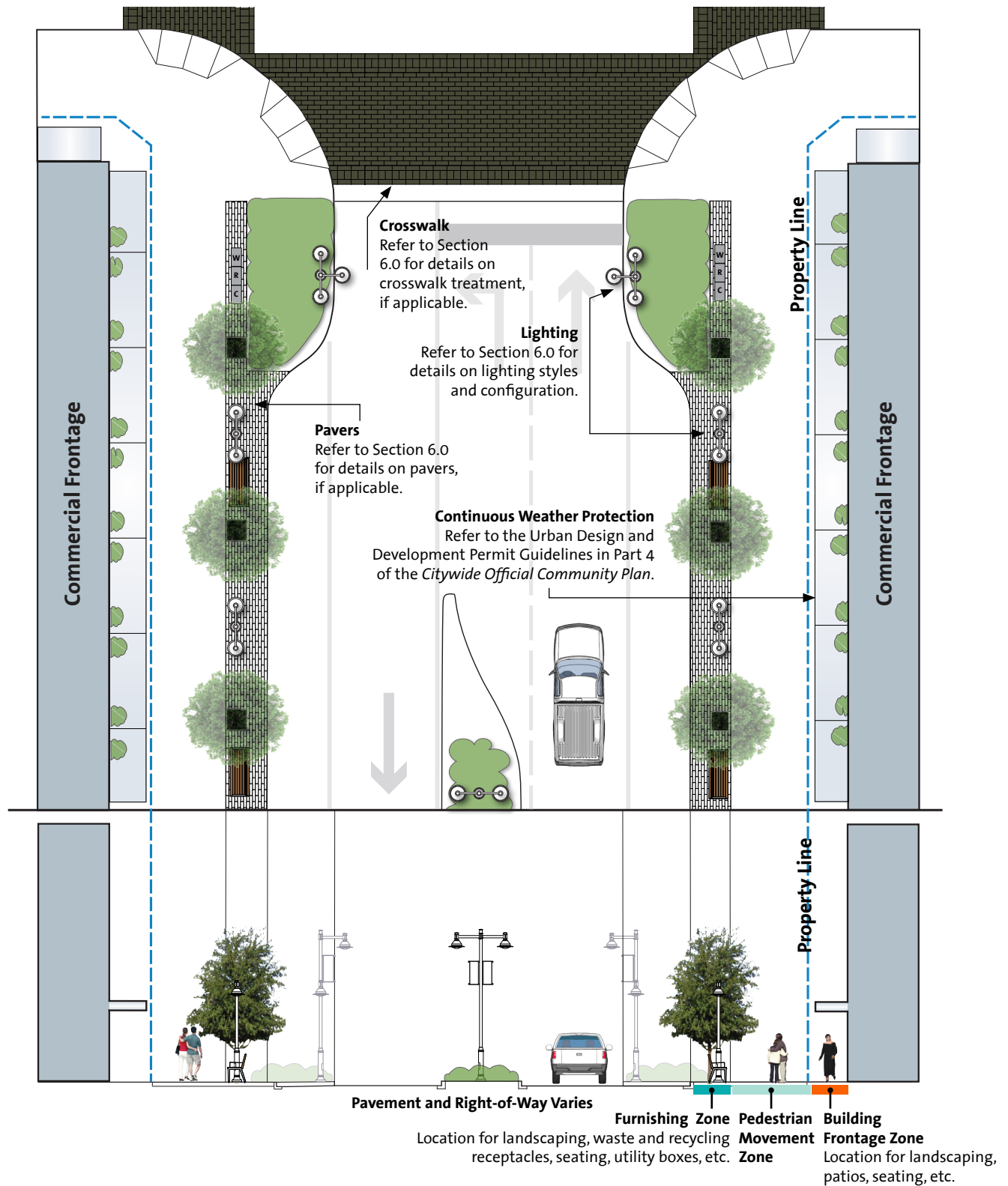


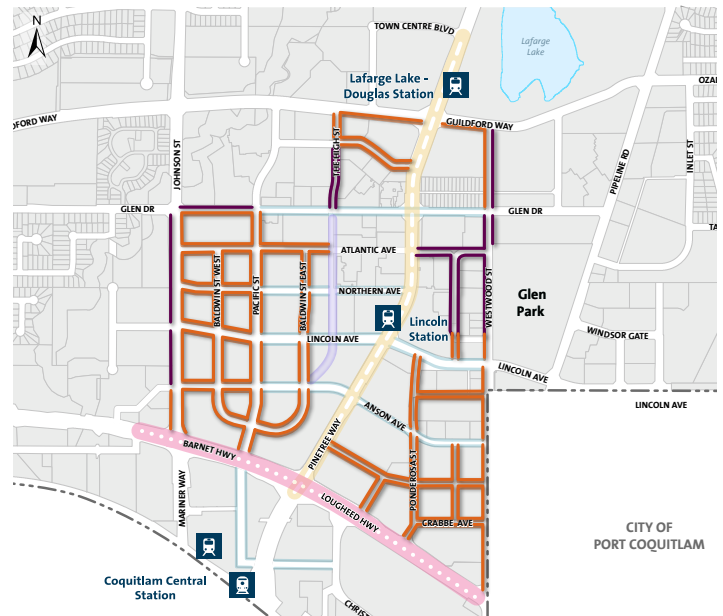
Figure 6 - Plan View and Cross-Section of Commercial Fronting Streets.

Notes:

1. Drawing not to scale. Provided for illustrative purposes only.
2. Refer to Appendix A for full cross-sections.

3.1.2 Residential Frontages

Residential frontages apply to streets designated **Residential Street Frontage** and **Secondary Active Street Frontage**. These streets are characterized by having soft landscaping such as grass, trees, shrubs, flowers or other plants in the **Boulevard Zone** to support a character best suited for residential areas.



Residential Street Frontages and Secondary Active Street Frontages.



*Conceptual sketch of a Residential Street Frontage or Secondary Active Street Frontage
(Artist's concept - It will not be built exactly as shown).*

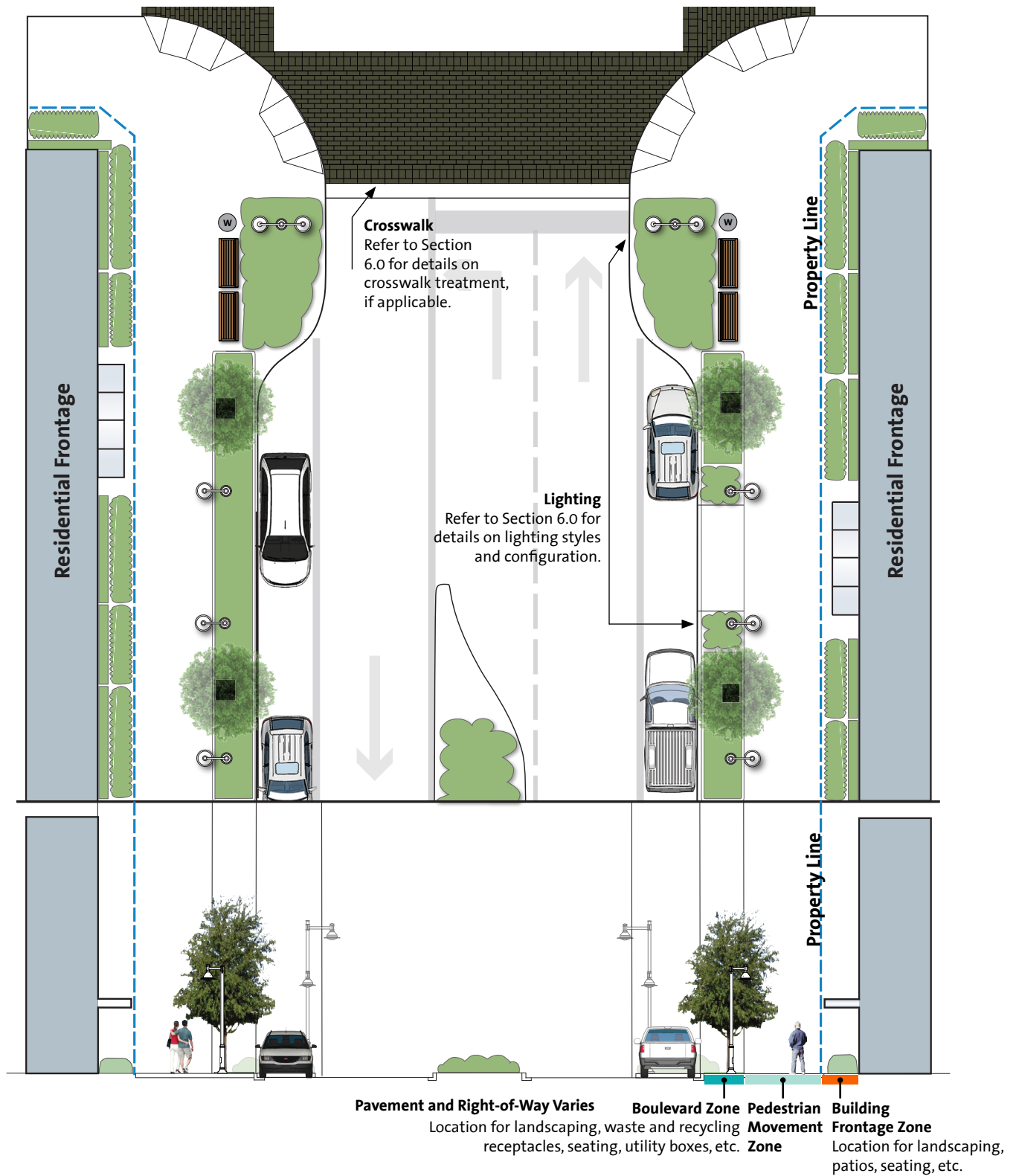


Figure 7 - Plan View and Cross-Section of Residential Fronting Streets.

Notes:

1. Drawing not to scale. Provided for illustrative purposes only.
2. Refer to Appendix A for full cross-sections.

3.1.3 Pinetree Way Character Street

The Pinetree Way Character Street currently features an enhanced multi-modal streetscape with pedestrian-friendly lighting, unique custom street furniture, public art and wayfinding signage to encourage people to walk, cycle, roll and use transit. The streetscape design of the Pinetree Way Character Street will continue to demonstrate urban design excellence and reinforce the multi-modal nature of the street with micromobility facilities, and provide individuals with convenient access to major destinations in City Centre.

Landscaping and dynamic lighting under the SkyTrain guideway will soften the streetscape, enhance safety and create a sense of place. Additional landscaping and trees along the street will reinforce Coquitlam's connection to nature and watercourses through green infrastructure as well as public art and unique paving patterns which give the impression of moving water.



Pinetree Way Character Street.



Conceptual sketch of the Pinetree Way Character Street (Artist's concept - It will not be built exactly as shown).

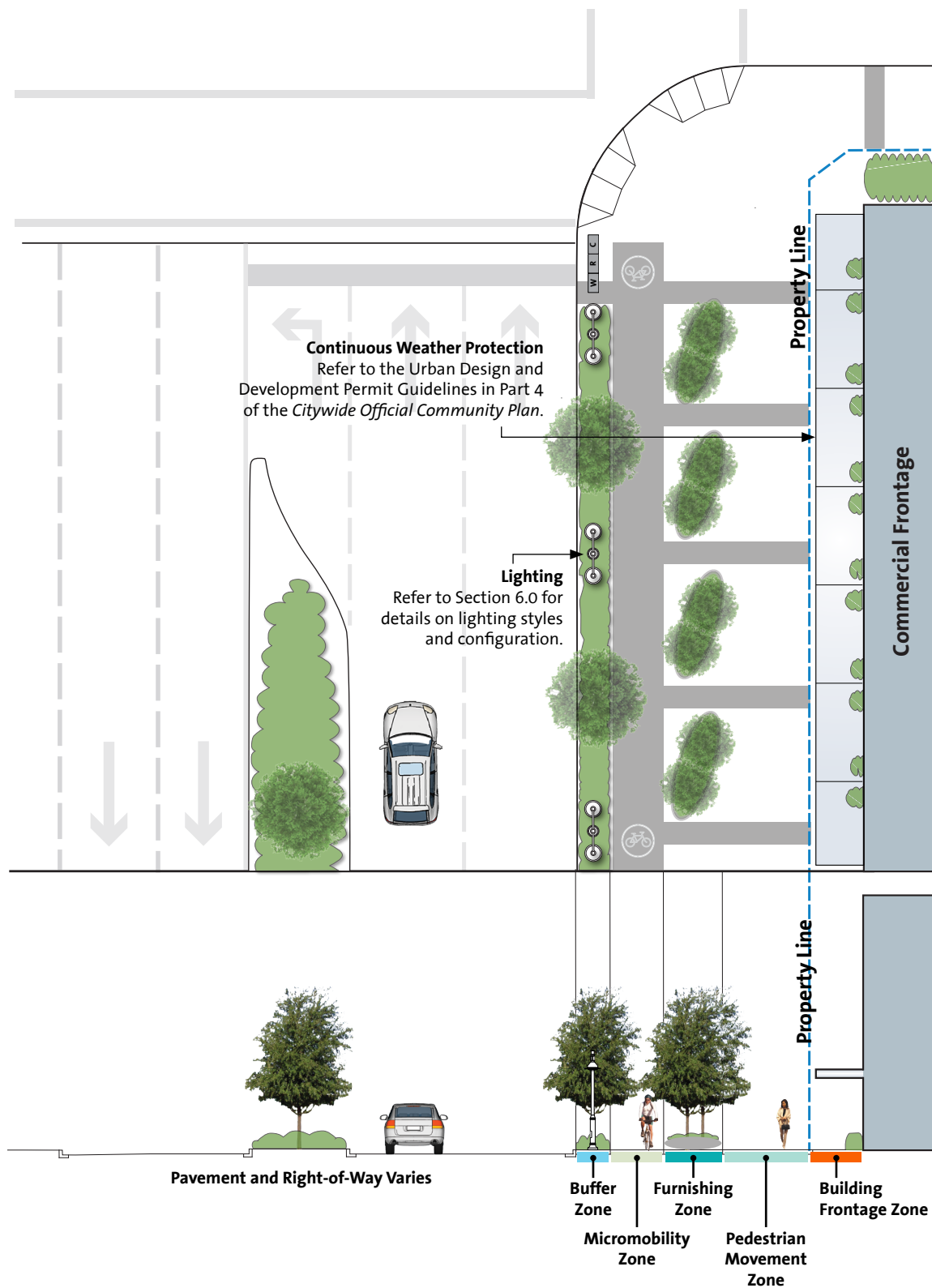


Figure 8 - Plan View and Cross-Section of the Pinetree Way Character Street.

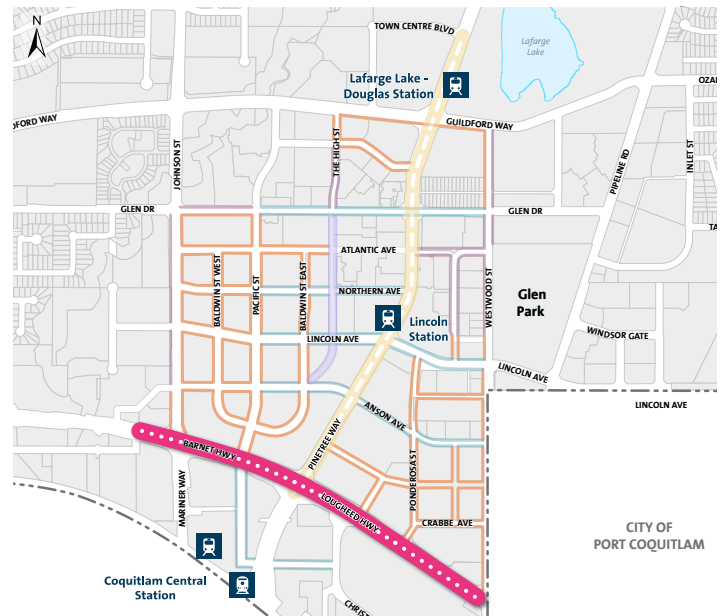
Notes:

1. Drawing not to scale. Provided for illustrative purposes only.
2. Refer to Appendix A for full cross-section.
3. Where possible, the Micromobility Zone should be protected at intersections, as illustrated on Figure 9.

3.1.4 “Urban Boulevard” Character Street

Through redevelopment, the “Urban Boulevard” Character Street will demonstrate design excellence and transform into a pedestrian and micromobility friendly boulevard to provide enjoyable connections to key destinations. High-quality landscaping, green infrastructure and a double row of street trees will soften the streetscape while providing pedestrians and micromobility users a comfortable buffer from vehicle travel lanes. The public realm will reinforce Coquitlam’s connection to nature and water by bringing elements of the area’s mountains, forests, watercourses and the Coquitlam River through the design of the streetscape.

Other elements such as unique lighting features, public art and seating will contribute to the success of the active frontages and support pedestrian interest, safety and visibility. Together, these elements will enhance the appearance of the Character Street and contribute to a welcoming arrival into City Centre.



“Urban Boulevard” Character Street.



Conceptual sketch of the “Urban Boulevard” Character Street (Artist’s concept - It will not be built exactly as shown).

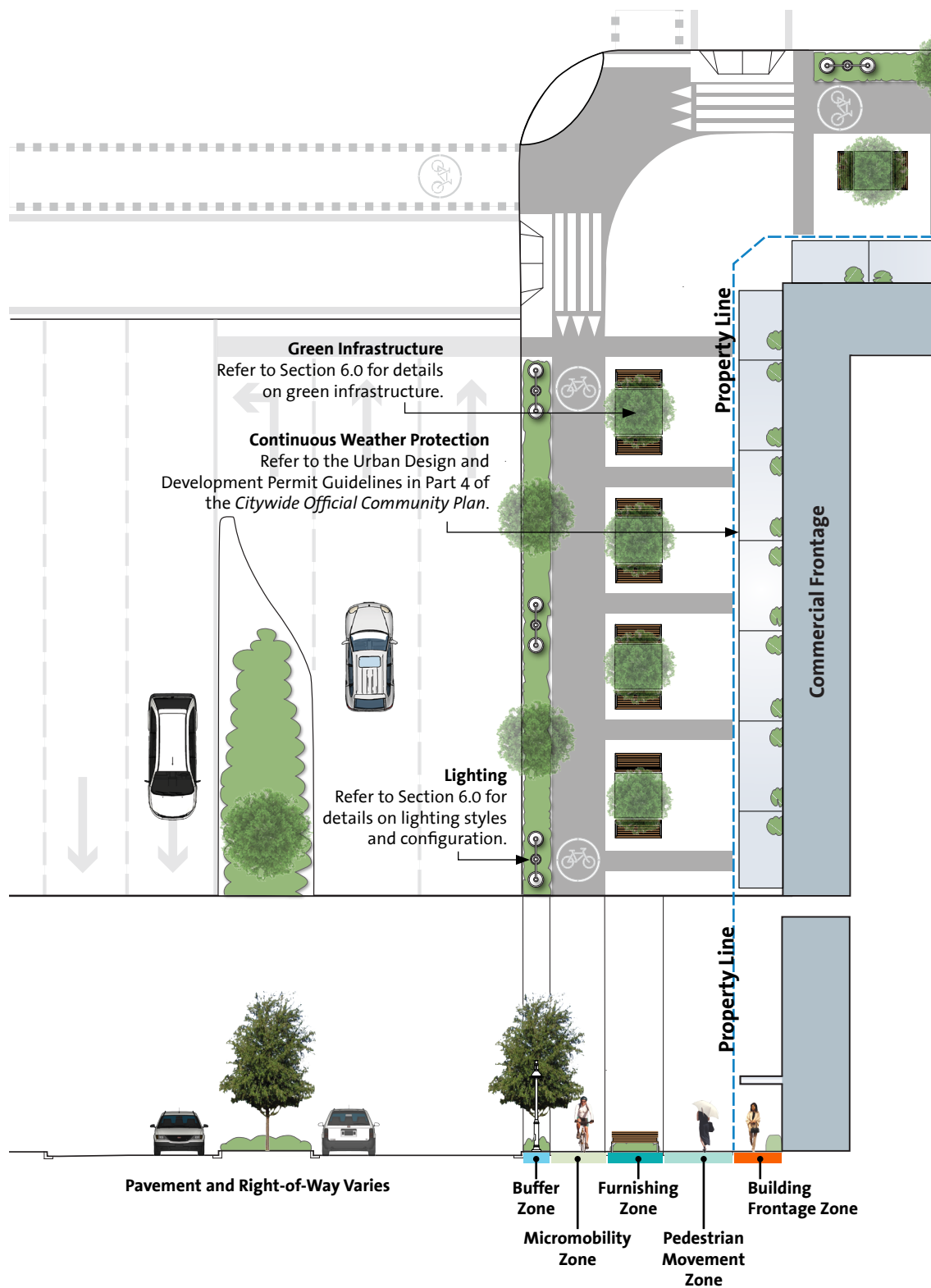


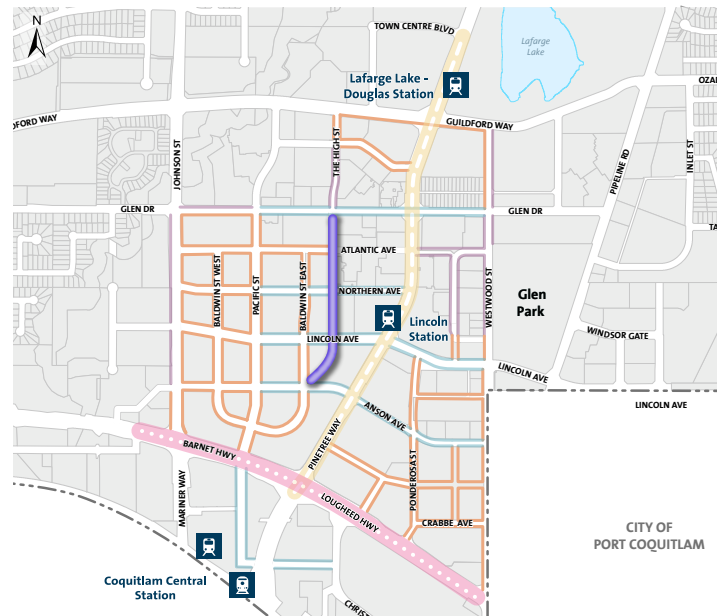
Figure 9 - Plan View and Cross-Section of the "Urban Boulevard" Character Street.

Notes:

1. Drawing not to scale. Provided for illustrative purposes only.
2. Refer to Appendix A for full cross-section.
3. Where possible, the Micromobility Zone should be protected at intersections.

3.1.5 “Downtown Promenade” Character Street

The public realm design for the “Downtown Promenade” Character Street will allow for a variety of programming and be designed as a destination that draws and anchors people to the space, during different times of the day and year. Outdoor lighting, public art and unique street furniture will support placemaking activities, socialization and celebrations. The streetscape design will accommodate individuals of all ages and abilities and include unique landscaping with trees, seasonal plants and stormwater management features such as rain gardens to create an environmentally-sustainable public realm.



“Downtown Promenade” Character Street.



Conceptual Sketch of the “Downtown Promenade” Character Street (Artist’s concept - It will not be built exactly as shown).

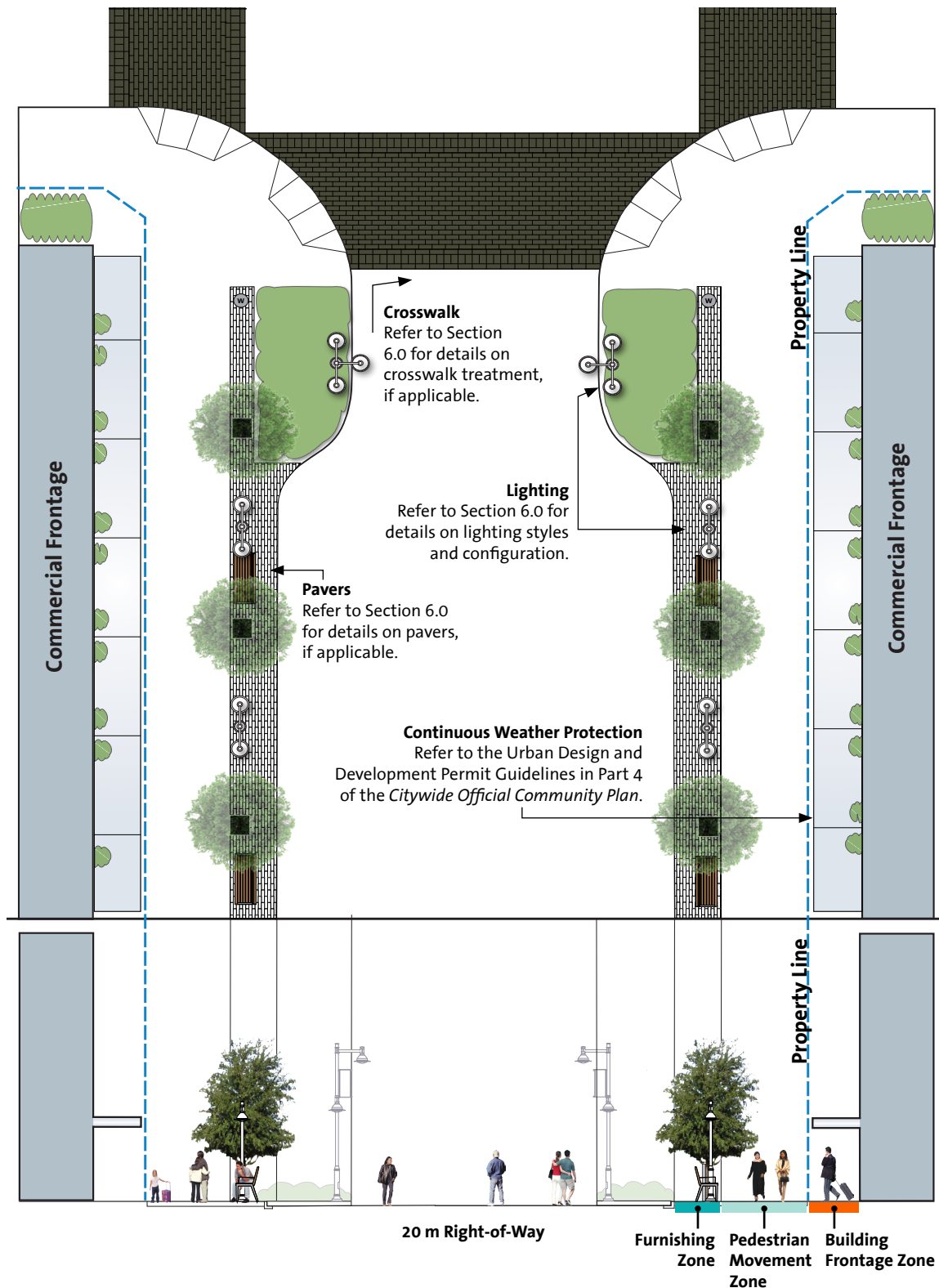


Figure 10 - Plan View and Cross-Section of the "Downtown Promenade" Character Street.

Notes:

1. Drawing not to scale. Provided for illustrative purposes only.
2. Refer to Appendix A for full cross-section.
3. A comprehensive review of the detailed design will be required to ensure a unique, innovative and high-quality streetscape design that supports the Entertainment District, Office Business District, and Civic Facilities and Parks.

3.2 Public Realm Guidelines

The following design guidelines apply to streets with a designated street frontage type or Character Street, as identified on **Figure 5** and Schedule I of the CCAP.

3.2.1 Guidelines for All Street Frontage Types and Character Streets

Street Cross-Sections

- a. The width of the **Building Frontage Zone** will be guided by the standards set in the *Zoning Bylaw*. The width of the **Pedestrian Movement Zone**, **Furnishing Zone**, **Boulevard Zone**, **Micromobility Zone** and **Buffer Zone** are outlined in Section 5 and Appendix A.

Commercial Frontages

- b. The **Furnishing Zone** will be covered in pavers. Where landscaping is provided in the **Furnishing Zone**, a hard surface treatment, will be provided next to the curb (total of 0.6 metres), to accommodate vehicle door openings and access to the **Pedestrian Movement Zone**.
- c. Provide visible commercial frontages to create transparency to the street, an active public realm and a sense of place that encourages people to linger.
- d. The **Building Frontage Zone** will include space for outdoor seating associated with cafes and restaurants, and outside merchandise display to encourage business activity, a vibrant street life and prevent doors from opening into the **Pedestrian Movement Zone**. In no case shall spill out spaces impede the **Pedestrian Movement Zone**.
- e. The **Building Frontage Zone** may be used as a secondary seating area for elements of the streetscape that are usually in the **Furnishing Zone** such as seating and parking pay kiosks where they do not compromise access to businesses and building entries or conflict with key design policies or objectives.

Residential Frontages

- f. The **Boulevard Zone** will be covered in soft landscaping. Plant selection will account for localized micro-climates.
- g. Provide soft landscaping and patios, where possible in the **Building Frontage Zone**. These elements will create a buffer from the street, avoid blank walls, provide eyes on the streets for safety, and create a place between the public and private realm that residents can activate to support a vibrant streetscape while maintaining their privacy.



Space for door openings in Building Frontage Zone with landscaping to separate the space from the Pedestrian Movement Zone.



Space for seating in the Building Frontage Zone to support commercial activity and a vibrant public realm along a commercial frontage.



Soft landscaping is provided in the Building Frontage Zone and Boulevard Zone to support a residential frontage character.

Frontage Transition

- h. Provide appropriate transition through soft landscaping and setbacks in areas where commercial and residential frontages are adjacent.

Street Furniture

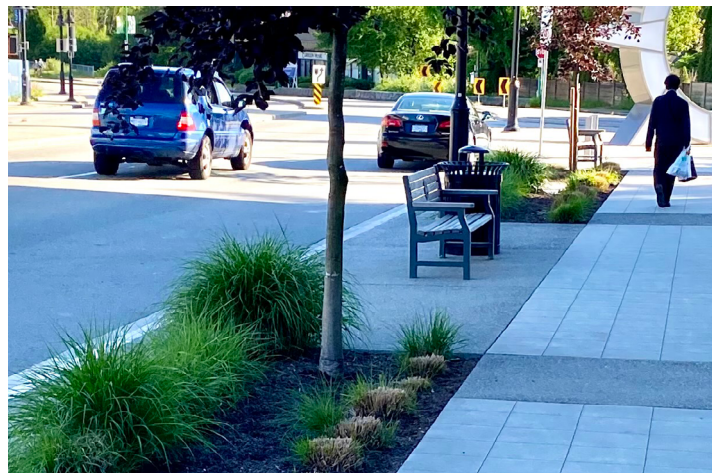
- i. Street trees and street furniture such as seating, bike racks, light poles, and waste and recycling receptacles will be located in the **Furnishing / Boulevard Zone** to ensure a safe public realm for those with mobility and vision impairments. In no case shall street trees or street furniture encroach into the **Pedestrian Movement Zone**, impede accessibility or create barriers for pedestrian circulation.
- j. Street furniture such as seating will be located away from the curb in the **Furnishing / Boulevard Zone** to provide sufficient space from moving vehicles and for vehicle door openings, where on-street parking is provided.
- k. Street furniture such as seating, bike racks, waste and recycling receptacles and parking pay kiosks should be strategically and thoughtfully grouped together in **Furnishing / Boulevard Zone** in a manner that is functional and attractive.
- l. Seating will be oriented (i.e. to the street or building façade) in a manner which considers the safety, comfort and enjoyment of the user.
- m. Bike racks will be located in well-lit areas and placed either in the **Building Frontage Zone** or in the **Furnishing / Boulevard Zone** under weather protection. Bicycle racks should also be sited so that bicycles, when parked, do not conflict with door openings or intrude into the **Pedestrian Movement Zone**.
- n. Streetscapes near and adjacent to parks, schools, urban trails, greenways and other areas of high pedestrian activity shall provide bear/wildlife-proof waste and recycling receptacles as guided by the standards set in the *Wildlife and Vector Control Bylaw* and as guided by the City's Bear Smart Certification.

Wayfinding Signage

- o. Provide wayfinding signage to help residents and visitors easily navigate to their destination in accordance with the *City Wayfinding Plan*.



Street trees and light poles are located outside of the Pedestrian Movement Zone.



Street furniture and landscaping are set back from the curb and a break in landscaping offers a barrier-free access from the curb to the Furnishing and Pedestrian Movement Zones.



Bike racks located under weather protection in the Furnishing Zone.

Public Art

- p. Integrate and promote public enhancements or public art as part of all new development in accordance with the *Public Art Policy and Project Plan*.
- q. Public art will be located either within the **Furnishing / Boulevard Zone** where space is adequate such as at curb bulges, or within the **Building Frontage Zone** where it will not conflict with building entries or other desire lines. In no case shall public art encroach into the **Pedestrian Movement Zone**, impede accessibility or create barriers for pedestrian circulation.

Pedestrians Crossings

- r. Reduce the pedestrian crossing distance through the use of curb bulges at intersections or mid-block crossings where walkways are present or on long blocks for the comfort of pedestrians of all ages and abilities.
- s. Provide decorative crosswalks at key intersections to reinforce and indicate pedestrian priority within the public realm. Refer to Section 6.0 for further details.

On-Street Parking

- t. Where public on-street parking and vehicle lay-bys are provided, planted bulges are encouraged where sufficient road width exists and other demands, such as transit stops, utility kiosks and related amenities and priorities of the streetscape and City bylaws, can be upheld.

Lighting

- u. Lighting in all areas will consider and enhance the safety and comfort of all individuals at all times of day.

Landscaping and Street Trees

- v. Plants in the **Building Frontage Zone** and **Furnishing / Boulevard Zone** will be selected to not impede pedestrian movement or sightlines for all street users.
- w. Along commercial fronting streets where soft landscaping is provided in the **Furnishing Zone**, provide a barrier-free access from the curb to the **Pedestrian Movement Zone** using a hard surface treatment. This does not apply to residential fronting streets.
- x. The minimum distance between street trees and clearance from elements such as streetlights, street intersections, driveways and kiosks will be guided by the *Subdivision and Development Servicing Bylaw*.
- y. Where possible, medians should be widened to accommodate street trees and landscaping.



Planted curb bulges to enhance the streetscape and shorten pedestrian crossings.



Pedestrian-oriented lighting to create a welcoming, safe and comfortable public realm.



Landscaping to create pedestrian interest.

Urban Design

- z. Make reference to Coquitlam's various historical industries through the use of unique materials in the **Building Frontage Zone** or **Furnishing / Boulevard Zone** while ensuring universal accessibility. Examples include steel, wood and exposed aggregate.
- aa. Reinforce Coquitlam's connection to its natural setting of mountains, forests, watercourses and the Coquitlam River through the design and use of materials in the landscaping and streetscape design. Examples of designs representing the area's natural elements include salmon, trees, mountains, forests and watercourses.

Transit Shelters

- ab. Transit shelters will be integrated in the streetscape, where feasible, and enhanced with pedestrian amenities such as lighting, seating, waste and recycling receptacles, and weather protection.

Mechanical Systems, Utilities and Service Infrastructure

- ac. All equipment and components associated with mechanical systems, utilities, and services will be located underground and on the subject site whenever possible. Where they cannot be located underground or on the subject site, they will be designed to minimize adverse impacts on the public realm and nearby properties. They will also be located, oriented, screened and wrapped as required to minimize negative visual and acoustical impacts. In no case shall infrastructure encroach into the **Pedestrian Movement Zone**, impede accessibility or create barriers for pedestrian circulation.
- ad. Where above-grade equipment or associated components are required within the right-of-way, they must be located in the **Furnishing / Boulevard Zone** or in the **Building Frontage Zone** and be coordinated with planting and landscaping features. In no case shall infrastructure encroach into the **Pedestrian Movement Zone**, impede accessibility or create barriers for pedestrian circulation.
- ae. Where below-grade components such as vaults, air intake/exhaust shafts and other enclosures require access panels, hatches, lids and grates will be slip resistant, ADA compliant, durable, low-maintenance, attractive and flush with the adjacent surface. Where applicable, components that are highly visible may require enhanced or custom designs, particularly in important places or locations. In no case shall infrastructure encroach into the **Pedestrian Movement Zone**, impede accessibility or create barriers for pedestrian circulation.



Unique paving pattern and materials in Spirit Square reference Coquitlam's name which comes from the Coast Salish word kʷikʷəłəm (kwikwetlem) meaning "red fish up the river".



Transit shelter with weather protection, seating and waste receptacle.



Utility box effectively screened with landscaping and wrapped in graphic film to improve aesthetics of the public realm.

3.2.2 Additional Guidelines for All Character Streets

- a. Provide stormwater management such as rain gardens in the **Furnishing / Boulevard Zone** and **Buffer Zone** (where space permits) to enhance the natural environment, provide shade, and improve cleansing and filtration of stormwater, in accordance with the *Supplementary Specifications Master Municipal Construction Document*, *Coquitlam Green Development Guide* and *Metro Vancouver Stormwater Source Control Design Guidelines*, and other City standards and requirements.
- b. Provide a double row of street trees with required soil volumes to support mature tree growth as well as landscaping and an appropriate setback from vehicles to soften the streetscape and enhance the public realm experience for pedestrians and micromobility users on the Pinetree Way and “Urban Boulevard” Character Streets given the high-volume of moving vehicles.
- c. Provide streetscape infrastructure for banners and lighting features to highlight the Character Streets as special places with a unique identity.
- d. Provide custom street furniture and integrate lighting, where possible, to support placemaking activities, a vibrant public realm, enhance public safety and provide opportunities to pause and rest.



Integration of lighting in street furniture to enhance public safety and comfort at all times of the day.

3.2.3 Additional Guidelines for the “Downtown Promenade” Character Street

- a. Create a central and vibrant destination by exploring unique, innovative and high-quality streetscape designs.
- b. Explore opportunities to provide planters for year-round colour and visual interest.
- c. Provide unique site furnishings and materials that draw and anchor people to the space.
- d. Explore opportunities for a vehicle-free street for events, markets, festivals and outdoor activities between Northern Avenue and Anson Avenue with an emphasis on placemaking activities.
- e. Consider rollover or flush curbs to support the closure of the street.
- f. A comprehensive review of the detailed design will be required to ensure high-quality urban design, safety, universal accessibility, drainage, maintenance, street delineation, parking, street event programming and other needs are met.



Colourful planting in the public realm to create visual interest and connections to nature.



Unique seating furniture and lighting to create a welcoming and safe setting to gather, play and celebrate.

4.0 MICROMOBILITY, GREENWAYS AND LINEAR PARK



The Pinetree Way Character Street and Citywide Greenway under the SkyTrain guideway with lighting, landscaping, street trees and seating to provide opportunities to pause and rest.

4.1 Micromobility

Micromobility includes human-powered mobility devices (e.g., bikes, kick scooters) and electric assist mobility devices (e.g., electric bikes, electric kick scooters) which can be personally owned or used in shared fleets.

To support these emerging light transportation modes, and to supplement the greenway network already established in the CCAP, a network of micromobility facilities is being introduced in City Centre as illustrated on **Figure 11**. These facilities will help increase sustainable mode share within the city, help separate modes and provide a safer environment for all street users. The Strategic Transportation Plan (STP) update and integrated E-Mobility Strategy will further define micromobility and facility types across the city as well as include policy, planning and regulatory actions.

Where possible, micromobility facilities should be protected at intersections, as illustrated on **Figure 9**.



Micromobility user on a bicycle.

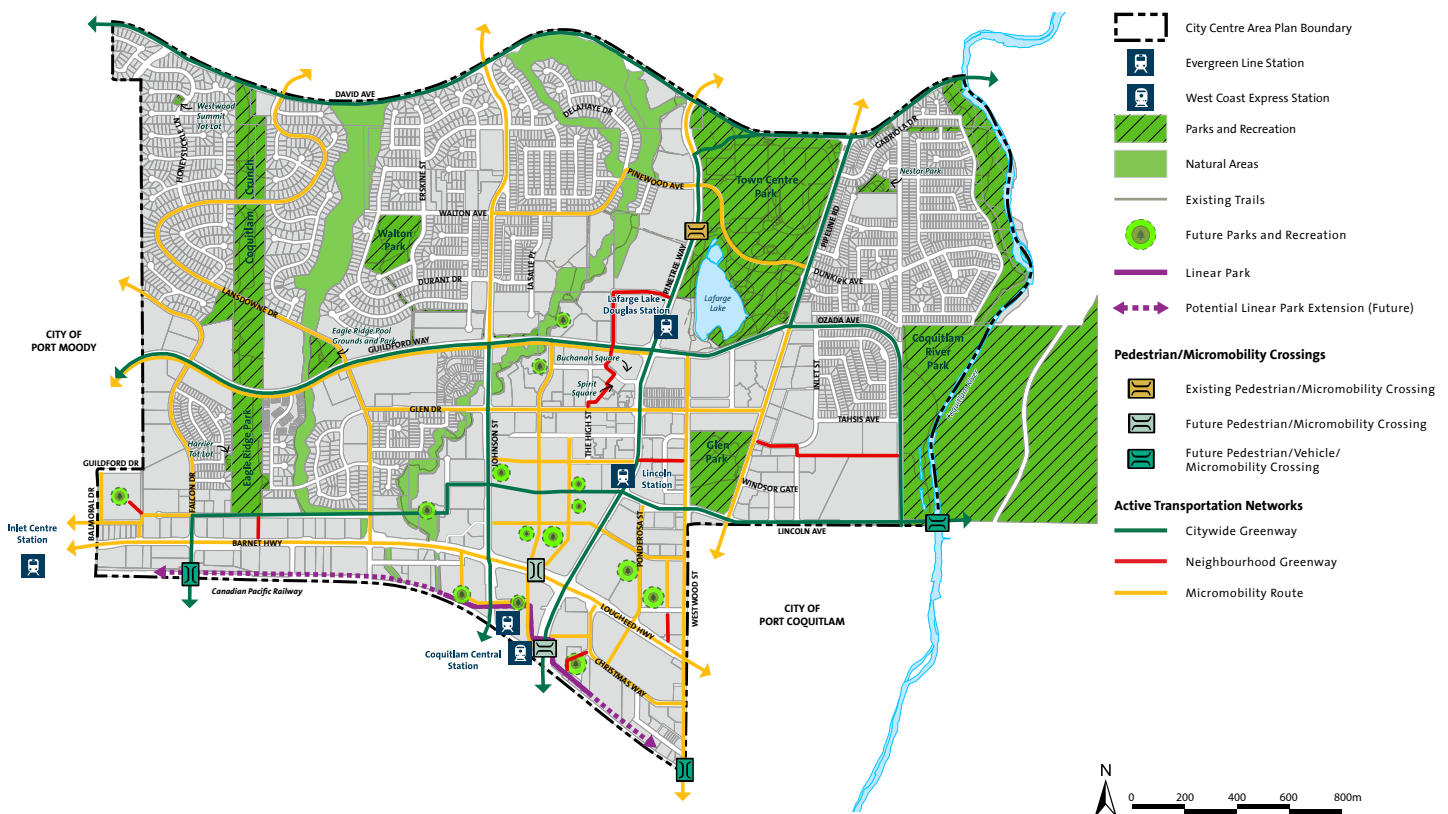


Figure 11 - Active Transportation Network.

Notes:

1. The Future Parks and Recreation sites as shown are conceptual and the actual location will be determined through future development applications. The final subdivision layout is subject to review by the City's Approving Officer.
2. Opportunities to extend Citywide Greenways may be considered as part of redevelopment and could include additional connections with other areas of Coquitlam, and neighbouring municipalities, while additional Neighbourhood Greenways may be identified as part of Master Development Plans to provide connections to local destinations.

4.2 Greenways

Greenways are key multi-modal corridors that are enhanced with landscaping, greenery and furnishing treatments. They encourage active modes of transportation, and support pedestrians and micromobility users for both recreational and commuting purposes.

The CCAP includes a comprehensive network of Citywide and Neighbourhood Greenways in the form of off-street pathways, as illustrated on **Figure 11** and Schedule G of the CCAP. Enhanced treatments distinguish these greenways from other active transportation routes in City Centre.

Citywide Greenways

Citywide Greenways are long, continuous routes for pedestrians and micromobility users that strategically link major destinations throughout the City, including commercial centres, schools, public parks, and other community facilities. Specific Citywide Greenway features will be explored based on available right-of-way and in conjunction with private development.

Neighbourhood Greenways

Neighbourhood Greenways are shorter connections provided in the City Centre to enhance pedestrian and micromobility connections, and shorten distances to local destinations. Neighbourhood Greenway routes will feature frontage improvements such as wider **Pedestrian Movement Zones**, landscaped **Boulevard Zones**, mid-block pedestrian crossings, and curb extensions where appropriate, as guided by the standards set in the *Subdivision and Development Servicing Bylaw*. Specific Neighbourhood Greenway features will be explored in conjunction with private development.

Citywide Greenways Design Guidelines:

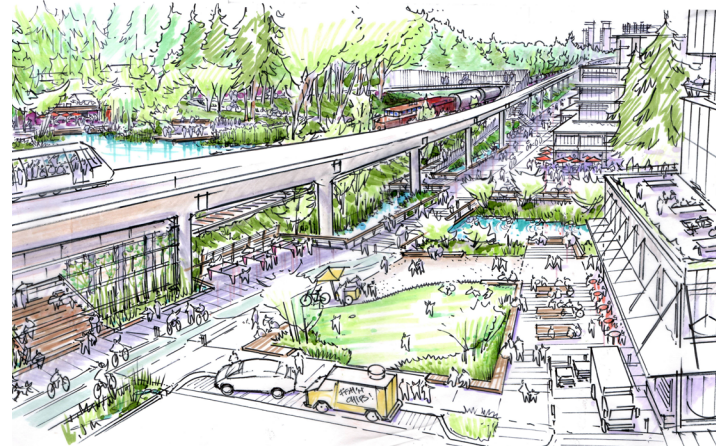
- a. Design standards for Citywide Greenways will be guided by the *City Centre Area Plan*, and other City standards and requirements.
- b. Establish green and natural corridors as an enjoyable place for both passive and active recreational opportunities in City Centre.
- c. Select architectural and landscaping elements including trees, vegetation and seating that are creative, contribute to a unique public realm and support placemaking.
- d. Develop clear visual cues to identify greenways as multi-modal routes within the neighbourhood and the city.
- e. Establish a uniform treatment of greenway surfacing and layout to reinforce the use and character.
- f. Where possible, reduce the pedestrian crossing distance through the use of curb bulges at intersections or mid-block crossings where walkways are present or on long blocks for the comfort of pedestrians and micromobility users.
- g. Where a Citywide Greenway is located outside of a street right-of-way, it shall have a minimum right-of-way of six metres, and possibly wider to provide physical separation between pedestrians and micromobility users.

4.3 Linear Park

A Linear Park is planned near the Pinetree-Lougheed Precinct and will serve as a gateway to City Centre. Along with other greenways, the Linear Park will create a connected recreation experience, support a vibrant and safe public realm, and improve access to public amenities such as new neighbourhood recreation centres, parks, natural areas and the Coquitlam Central station.

The location of the Linear Park is identified on **Figures 11 and 12** and Schedules E and G of the CCAP, and its design is guided by criteria outlined in the CCAP.

Figure 13 illustrates the cross-sections of the Linear Park.



Conceptual Sketch of the Linear Park (Artist's concept - It will not be built exactly as shown).

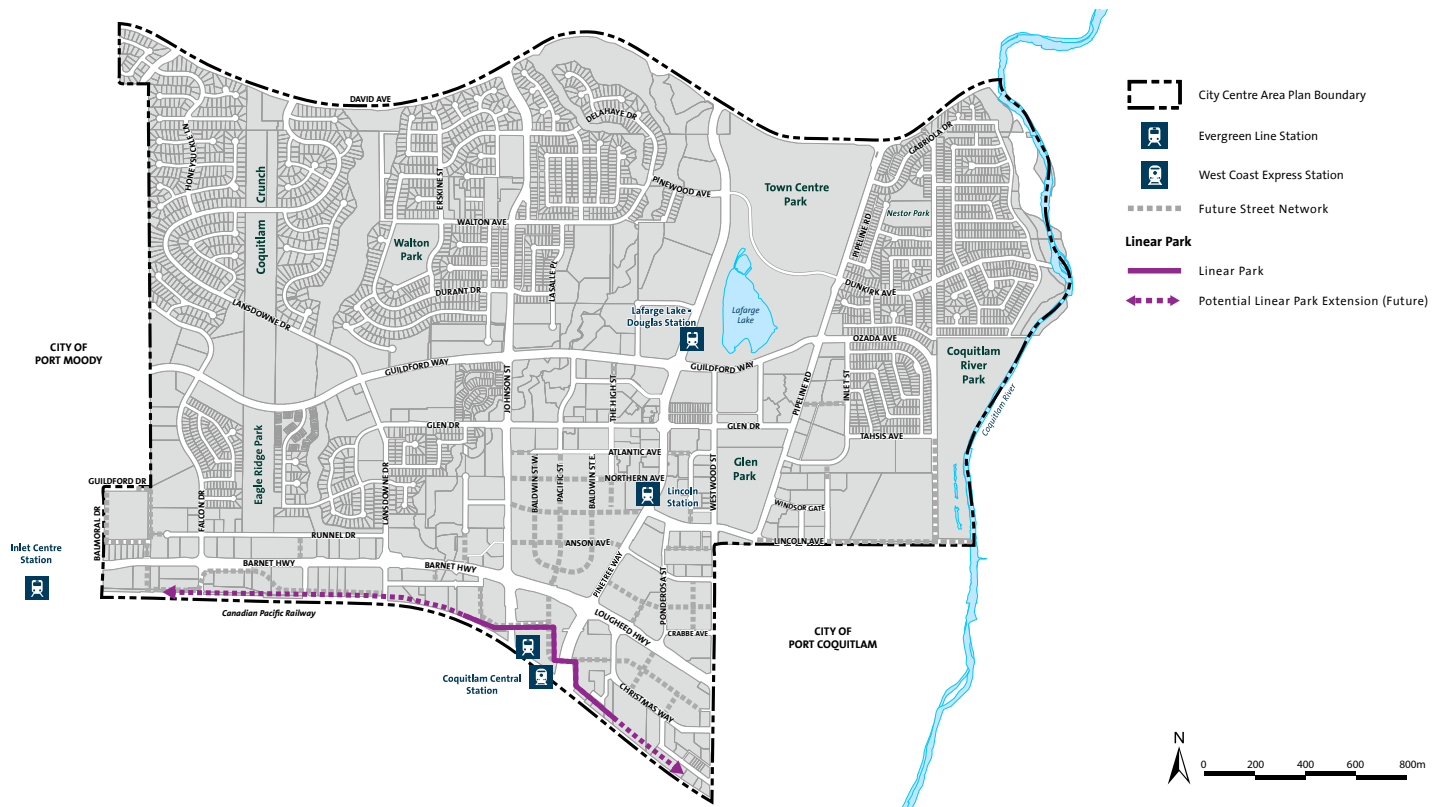


Figure 12 - Linear Park.

Note:

1. The Linear Park as shown is conceptual and the actual location will be determined through future development applications. The final subdivision layout is subject to review by the City's Approving Officer.
2. Opportunities to extend the Linear Parkway eastward and westward will be considered through a future parks planning process and could consider connections with other areas of Coquitlam, existing north-south recreational corridors and neighbouring municipalities.

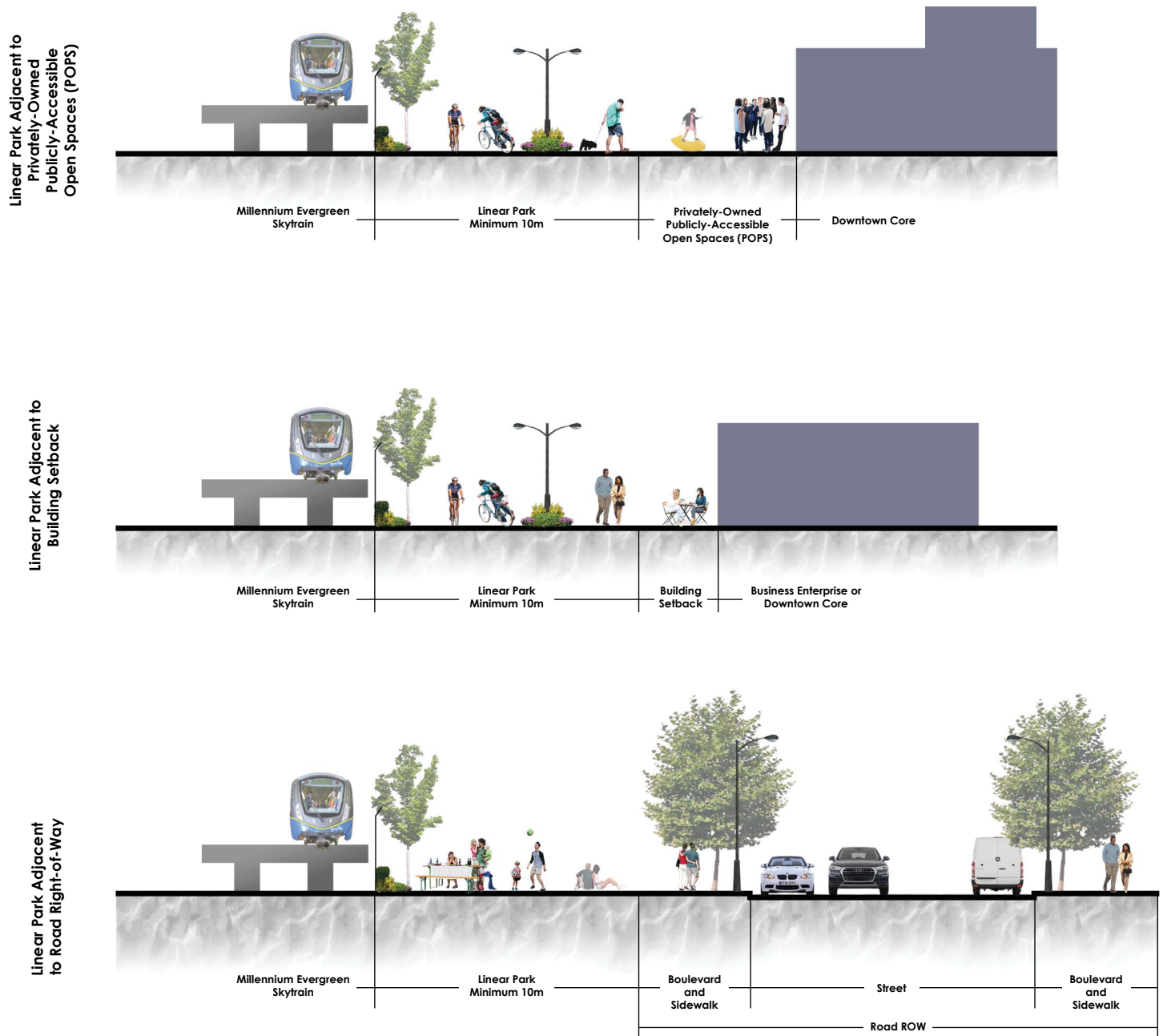


Figure 13 - Cross-Sections of the Linear Park.

Note:

1. Drawing not to scale. Provided for illustrative purposes only.

5.0 STREET CROSS-SECTIONS



Clear Pedestrian Movement Zone and landscaped Boulevard Zone.

5.1 Street Cross-Sections

To achieve the desired streetscape in the downtown, a set of midblock cross-sections have been developed for streets with a street frontage type or Character Street designation. These cross-sections provide details on the desired width of the streetscape zones as well as the road right-of-way for key streets in the downtown.

Table 1 summarizes the cross-sections for the streets identified in **Figure 14**. The full street cross-sections can be viewed in Appendix A. Streets not identified will be guided by the *Supplementary Specifications Master Municipal Construction Document*, and other City standards and requirements.

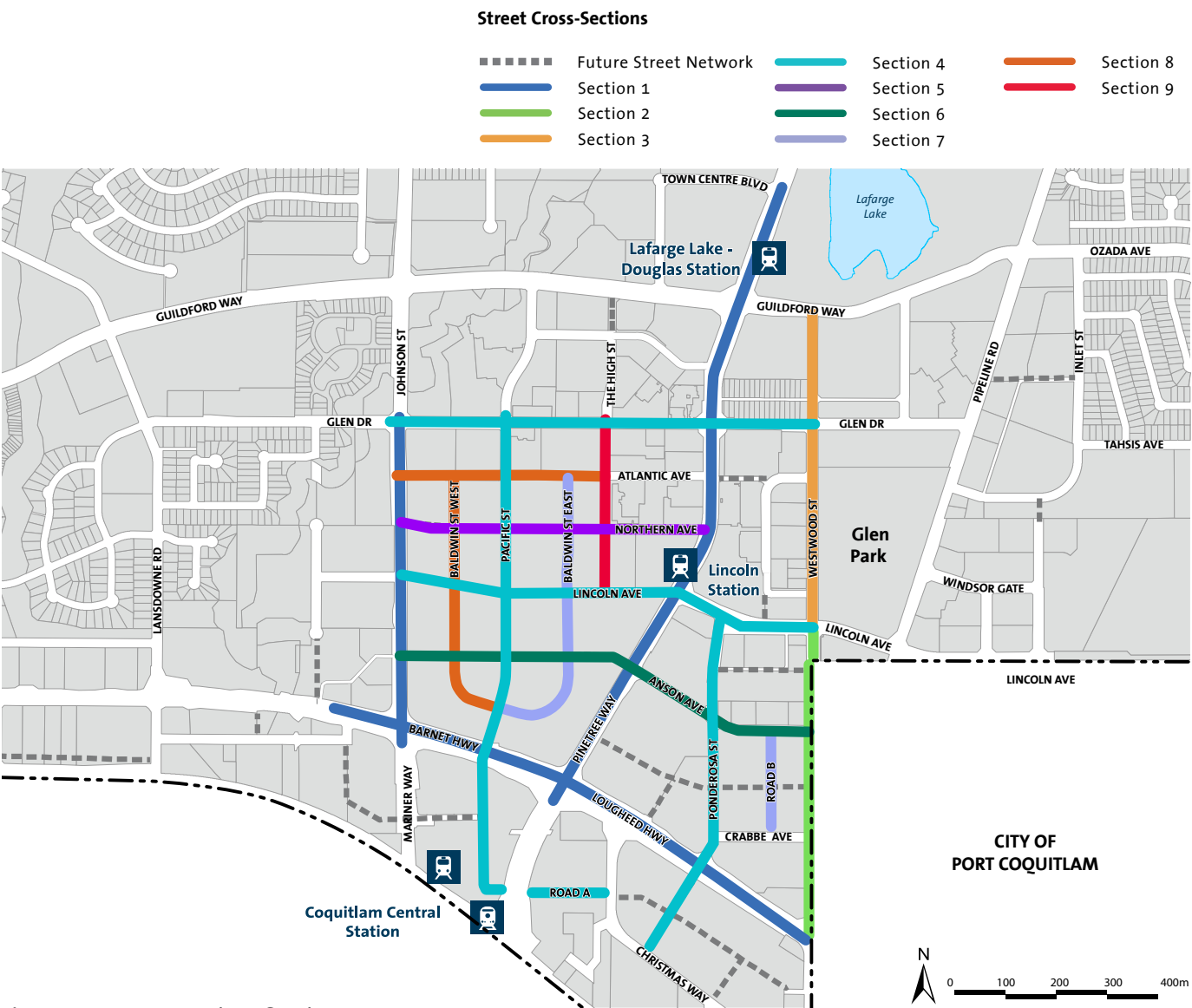


Figure 14 - Streets with Defined Cross-Sections.

Table 1 - Street Cross-Sections Summary.

Section	Street	Cross-Section
1	Lougheed Highway/ Barnet Highway Pinetree Way Johnson Street	2.5m x 2 Pedestrian Movement Zone 2.2m x 2 Boulevard/Furnishing Zone 2.5m x 2 Micromobility Zone 2.2m x 2 Buffer Zone Pavement and Right-of-Way (RoW) varies
2	Westwood Street (South of Lincoln Avenue)	4.0m west Multi-Use Pathway 2.5m west Boulevard/Furnishing Zone Pavement and RoW varies
3	Westwood Street (North of Lincoln Avenue)	2.5m west Pedestrian Movement Zone 4m east Multi-Use Pathway 2.5m x 2 Boulevard/Furnishing Zone Pavement and RoW varies
4	Glen Drive Pacific Street Lincoln Avenue (West of Pinetree Way) Ponderosa Street New Collector Road (Road A)	2.5m x 2 Pedestrian Movement Zone 2.2m x 2 Boulevard/Furnishing Zone 1.8 m x 2 Micromobility Zone 0.9m x 2 Buffer Zone 14m Pavement 28.8m RoW
5	Northern Avenue	2.5m north and 3.0m south Pedestrian Movement Zone 2.2m north and 2.5m south Boulevard/Furnishing Zone 1.8m x 2 Micromobility Zone 0.9m x 2 Buffer Zone 12m Pavement 27.6m RoW
6	Anson Avenue	2.5m x 2 Pedestrian Movement Zone 1.8m x 2 Micromobility Zone 2.2m x 2 Boulevard/Furnishing Zone 12m Pavement 25.0m RoW
7	Baldwin Street East New Local Road (Road B)	2.5m x 2 Pedestrian Movement Zone 1.8m x 2 Micromobility Zone 2.2m x 2 Boulevard/Furnishing Zone 10.5m Pavement 23.5m RoW
8	Atlantic Avenue Baldwin Street West	2.5m x 2 Pedestrian Movement Zone 2.2m x 2 Boulevard Zone/Furnishing Zone 10.5m Pavement 20m RoW
9	The High Street	3.0m x 2 Pedestrian Movement Zone 2.5m x 2 Boulevard Zone/Furnishing Zone 9.0m Pavement 20m RoW

Note:

1. Refer to Appendix A for complete drawings and details.
2. The cross-sections in Table 1 and Appendix A represent typical mid-block sections. Additional RoW may be required, depending on site-specific requirements (e.g., road widening for auxiliary lanes at intersections).
3. The Multi-Use Pathway on Westwood Street (North of Lincoln Avenue) will be designed in collaboration with Parks, Recreation, Culture and Facilities.

6.0 PUBLIC REALM FURNISHINGS



Landscaping and unique street furniture near the Lafarge Lake-Douglas SkyTrain Station.

6.1 Street Furniture

Street furniture in the public realm can enhance the setting of streets and public spaces by defining the space and creating a welcoming environment for people to rest, socialize and gather. Through thoughtful placement of street furniture in the public realm, places become more attractive and visitor-friendly and, in turn, encourage people to spend time outdoors while enjoying the surrounding environment and the experience it offers.

An appropriate selection of street furniture in the public realm also provides the elderly, individuals with mobility challenges, and families with young children a place to rest as they travel between destinations.

To create high-quality, safe and functional public spaces, the following should be considered when determining the placement and type of street furniture provided:

- The different destinations and uses (commercial or residential) at the street level;
- Thoughtful grouping of various street furniture;
- Consistency in the placement and design of street furniture; and
- Prioritization of universal accessibility and ensuring that placement does not impede pedestrian movement or accessibility.

Applicants are encouraged to explore unique street furniture designs that complement the surrounding facilities and uses. For example, providing unique benches that appear to be books near the Coquitlam Public Library at the City Centre Branch.








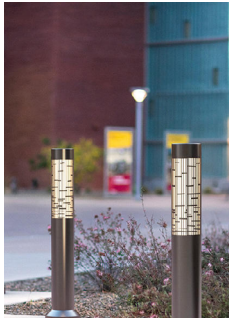

Unique pedestrian street markers along Pinetree Way Character Street.


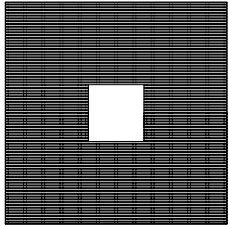


Decorative banners on light poles contributing to a sense of place.

Table 2 - Street Furniture.

Feature	Specifications	Mandatory Commercial Street Frontage	Secondary Active Street Frontage	Residential Street Frontage	Character Streets
Benches 	<p>Backed bench with armrests and wood slats.</p> <p>Supplier: Landscape Forms Model: Neoliviano Bench Size: 26.5" x 69" x 31" Finishes: Anodized aluminum finish for supports, unfinished Jarrah Accessories: 2 armrests</p>	Minimum four per block-face.	Minimum four per block-face.	Minimum four per block-face.	Minimum four per block-face.
Single-Stream Garbage Receptacles 	<p>Garbage receptacle with painted aluminum frame.</p> <p>Supplier: Landscape Forms Model: Lakeside Litter (Grass Side Open) Size: 21" x 36" Capacity: 30 Gal. Finish: Mercury powder coat (metallic)</p>	Minimum two per block-face.	Minimum two per block-face.	Minimum two per block-face.	Minimum four per block-face.
Multi-Stream Recycling Receptacles 	<p>3 stream 2-door recycling receptacle.</p> <p>Supplier: Envirozone Model: Hazelton Three EL-003 Dimensions: 23.6" x 70.8" x 47" Capacity: 39 Gal. Finish: Black powder coat Accessories: Board messaging</p>	Where required.	Where required.	Where required.	Where required.
Bike Racks 	<p>Stainless steel bike rack (supports two bicycles).</p> <p>Supplier: Landscape Forms Model: Bola Bike Rack Size: 1.42" x 27.42" x 32" Finish: Stainless Steel Installation: Embedded</p>	Minimum four per block-face.	Minimum four per block-face.	Minimum four per block-face.	Minimum four per block-face.

Feature	Specifications	Mandatory Commercial Street Frontage	Secondary Active Street Frontage	Residential Street Frontage	Character Streets
Fencing	Fencing required to control pedestrian movement should be painted metal in rectilinear forms with vertical dominating over horizontal (non-climbable).	Where required.	Where required.	Where required.	Where required.
Bollards 	Stainless steel bollard. Supplier: Reliance Foundry Model: R-8907 (removable and embedded option, as required) Height: 36" Weight: 32lbs Finish: Stainless steel Accessories: White reflective tape	Where required.	Where required.	Where required.	Where required, only along the Pinetree Way and "Urban Boulevard" Character Streets.
Bollards 	Stainless steel removable LED bollard. Supplier: Forms + Surfaces Model: Light Column Bollard (removable, LED) Shield Design: Strum (360° directional shield design) Height: 45" Diameter: 6" Finish: Stainless steel with ceramiloc				Where required, only along the "Downtown Promenade" Character Street.
Drinking Fountains 	Drinking fountain with bottle filler and pet fountain receptor. Supplier: Murdock MFG Model: GY Series GYM74-EG08-PF Height: 52" Color: Black Finish: Textured onyx black Accessories: Pet fountain receptor Installation: Include freeze resistant valves ADA Compliant: Yes	Minimum one approximately every 400m. Where possible, incorporate with Privately-Owned Publicly-Accessible Open Spaces.			Minimum one approximately every 400m. Where possible, incorporate with Privately-Owned Publicly-Accessible Open Spaces.

Feature	Specifications	Mandatory Commercial Street Frontage	Secondary Active Street Frontage	Residential Street Frontage	Character Streets
Pedestrian Street Markers 	<p>Decorative pedestrian street marker light column with custom made bark texture cladding.</p> <p>Supplier: Forms + Surfaces Type: Light Column Pedestrian LPLCO-512 Height: 132" Column Width: 5" Cladding Width: 7"</p>				One at every intersection along a Character Street.
Tree Grates 	<p>Decorative Tree Grate.</p> <p>Supplier: Dobney Foundry Style: SJ-48CAT Size: 48" Tree Opening: 12" Thickness: 1.5" Material: Ductile Iron Finish: Unfinished ADA Compliant: Yes Note: Tree grate to be cast in two mirror halves and designed with break-a-way-sections.</p>	Where tree is provided in hardscaped areas.	Where tree is provided in hardscaped areas.	Where tree is provided in hardscaped areas.	Where tree is provided in hardscaped areas.

Notes:

1. All street furniture should be galvanized to protect against corrosion.
2. A pedestrian-friendly block (defined to be between two cross streets) should be a targeted length of 200 metres (measured between the curb faces of two cross streets), where feasible as part of redevelopment.
3. Proposals for different bike racks shall be reviewed and approved by the General Manager of Engineering & Public Works.
4. Unique and high-quality street furniture, such as seating and tree grates and utility covers, should be explored as part for key commercial areas, Character Streets, and recreational and civic hubs.
5. The locations and frequency of site furnishing are to be determined during functional design on a case-by-case basis at the time of redevelopment. As a rule, the quantity and location of these furnishings should be generously applied, as reasonable and appropriate, and in concurrence with other perimeter works and services and as determined by the General Manager Planning & Development.
6. Applicants may propose additional streetscape design elements for each street frontage type and Character Street, and in some cases, the City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager of Planning & Development and General Manager of Engineer & Public Works.

6.2 Paving Materials and Treatments

The use of accent concrete banding, pavers and sandblasted designs in concrete can help create a unique public realm in areas of high pedestrian activity. To ensure superior construction, the elements described in this section will be guided by the standards set in the *Supplementary Specifications Master Municipal Construction Document*, and other City standards and requirements.

Construction drawings by a professional engineer and landscape architect showing how different concrete styles and pavers will tie into each other shall be reviewed and approved by the General Manager of Engineering & Public Works as part of the development process.

Universal Accessibility

All paving in publicly accessible areas will be chosen to maximize universal accessibility and the safety of those with mobility and vision impairment as well as the use of wheelchairs, canes, walkers, strollers and bicycles.

To ensure universal accessibility, the **Pedestrian Movement Zone** will be free of textural changes such as exposed aggregate or pavers. The **Micromobility Zone** will be in asphalt.

Along the Pinetree Way and “Urban Boulevard” Character Streets, the **Pedestrian Movement Zone** will have a medium sandblast concrete finish with accent banding, as illustrated in **Figure 15**. Concrete banding will align with the outside edges of the planters and the seating pockets. They will be spaced in a manner that is consistent with the overall street design. The design use should be harmonious, aesthetically appealing and shall not impede accessibility.

Paver and Street Furniture Installation

Along Mandatory Commercial Street Frontages, Secondary Active Street Frontages with commercial frontages and the “Downtown Promenade” Character Street, the **Furnishing Zone** will be covered in pavers installed over a concrete base, as illustrated in **Figure 16**.

In these areas, street furniture will have a finish base of pavers and appropriate anchoring below the pavers to ensure their stability and avoid overturning. Tree grates will have a cast iron frame surrounding the pavers to maintain the aesthetics of the streetscape.

Paver specifications are outlined in **Table 3**.



Smooth concrete in the Pedestrian Movement Zone and a combination of pavers and landscaping in the Furnishing Zone.



Bench installation with a finish base of pavers and appropriate anchoring below.



Tree grate installation surrounded by pavers with cast iron frame.

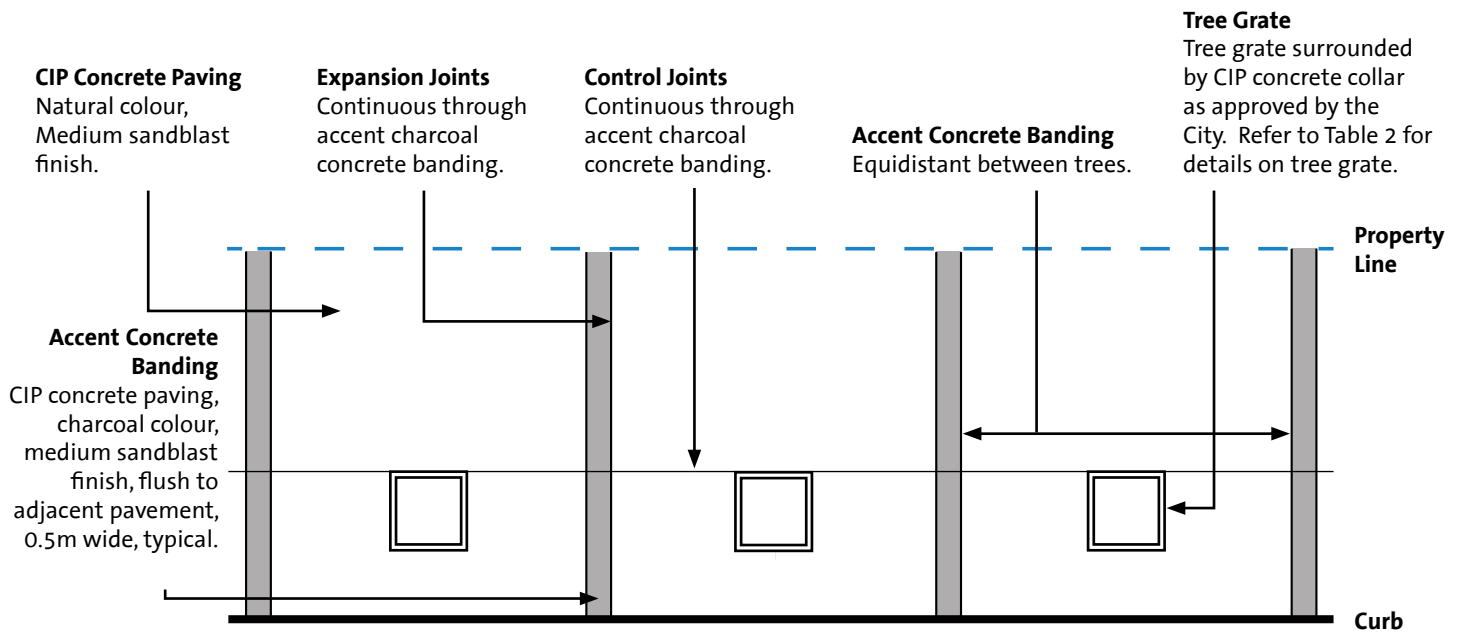


Figure 15 - Concrete Pattern along the Pinetree Way and “Urban Boulevard” Character Streets.

Note:

1. Drawing not to scale. Provided for illustrative purposes only.

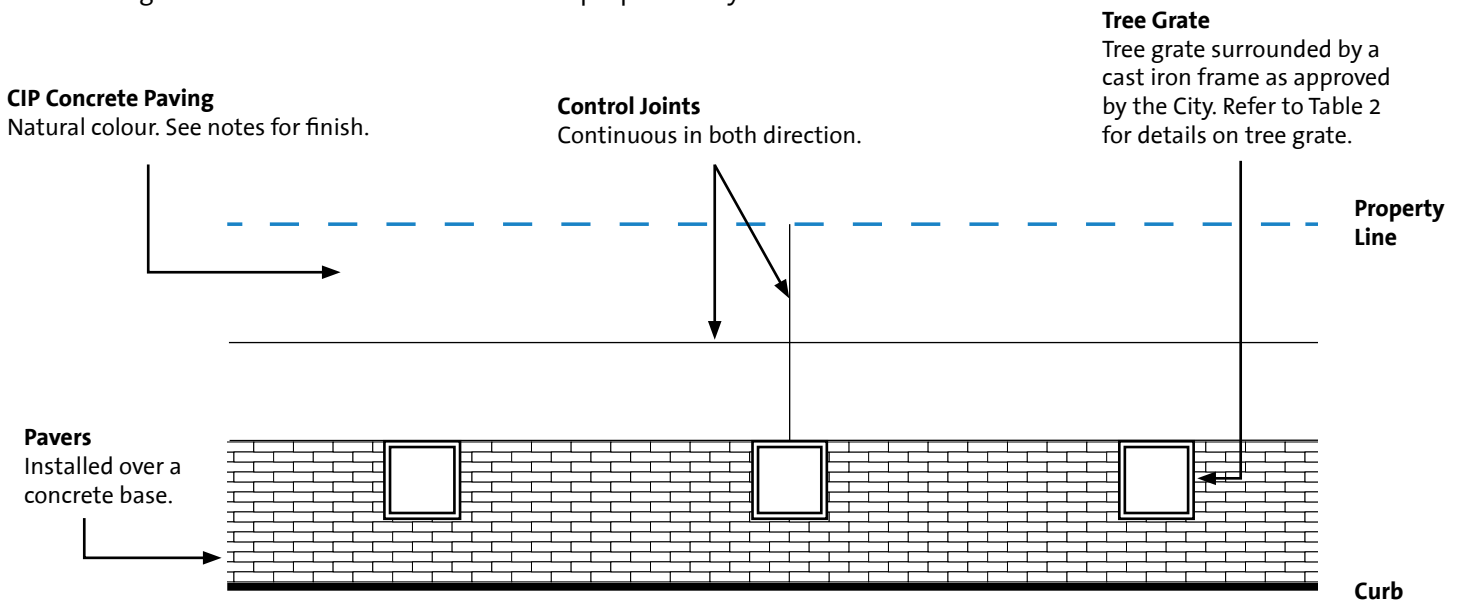


Figure 16 - Pavers along Mandatory Commercial Street Frontage Types, Secondary Active Street Frontage Types With Commercial Frontages and “Downtown Promenade” Character Street.

Notes:

1. Drawing not to scale. Provided for illustrative purposes only.
2. Mandatory Commercial Street Frontage Types, Secondary Active Street Frontage Types and Residential Street Frontage Types will have a soft broom finish for the Pedestrian Movement Zone.
3. The “Downtown Promenade” Character Street will have a medium sandblast finish for the Pedestrian Movement Zone.
4. Residential fronting streets will have a landscaped Boulevard Zone.

Buffer Zone Treatment

Where micromobility is required, the **Buffer Zone** between the curb and the **Micromobility Zone** will be landscaped. Where landscaping is not possible, provide pavers or a combination of pavers and landscaping.

Where the **Micromobility Zone** is directly adjacent to the **Pedestrian Movement Zone**, a single row of Mesa Brown 75mm x 451mm paver will be installed on a concrete base to visually separate both zones. The paver width shall form part of the **Micromobility Zone**. Paver specifications are outlined in **Table 3**.

Crosswalk Treatment

At key intersections, pedestrian crosswalks will have a stamped surfacing with a durable preformed thermoplastic material as specified in **Table 3**, to reinforce and indicate pedestrian priority within the public realm. Applicants are encouraged to explore unique designs that are reflective of Coquitlam.

Regulatory white crosswalk lines (100mm wide) are required on the outside of crosswalks.

Greenway and Linear Park Pavement Treatment

Unique decorative designs will be explored along Citywide Greenways and the Linear Park to create a unique sense of place and visual interest.

Opportunities for distinctive decorative designs that connect City Centre with Coquitlam's history, identity and natural setting will be explored during implementation. Motifs, such as salmon, watercourses, leaves, trees, or mountains, can be sandblasted on concrete or applied with a durable preformed thermoplastic material to asphalt.



Decorative crosswalk treatment to indicate pedestrian priority.



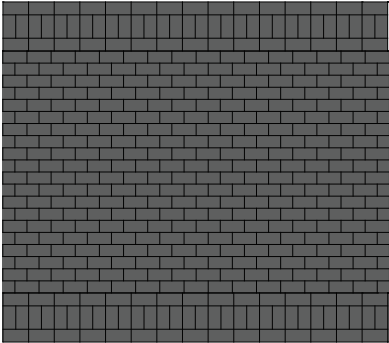


Unique pavement design on the Pinetree Way Character Street and Citywide Greenway giving the impression of moving water.



Unique public art and materials used to create wayfinding signage on the sidewalk at the Lafarge Lake - Douglas SkyTrain Station.

Table 3 - Paver and Crosswalk Specifications.

Feature	Specifications
<p>Furnishing Zone Pavers</p> 	<p>Standard pavers facilitating a linear laying pattern resulting in a finished and sleek appearance.</p> <p>Supplier: Barkman Model: Broadway Planks Installation: Installed over a concrete base Field pattern: Random Colours: Mesa Brown, Charcoal, Natural Size: 75mm x 451mm</p>
<p>Buffer Zone Pavers</p> 	<p>Longer than standard pavers facilitating a linear laying pattern resulting in a finished and sleek appearance.</p> <p>Supplier: Barkman Model: Broadway Planks Installation: Installed over a concrete base Field pattern: Random Colours: Mesa Brown, Charcoal, Natural Size: 150mm x 600mm</p>
<p>Crosswalks</p> 	<p>Enhanced stamped surfacing with a durable preformed thermoplastic material.</p> <p>Supplier: SquareOne Product: Traffic Patterns XD Pattern: Offset Brick (10-101) with Soldier Course border (18-1-199) Centre and border finish: Field Gray Regulatory border: Regulatory white crosswalk lines (100mm wide) outside of the Field Gray border.</p>

6.3 Decorative Lighting

Lighting plays a key role in the public realm by creating a safe experience for pedestrians, animating streets and contributing to a sense of place. Lighting elements in City Centre will use energy efficient and durable poles, and current technology luminaire options.

Decorative Lighting Areas

Decorative lighting applies to streets with a designated street frontage type or Character Street, as identified on **Figure 5** and Schedule I of the CCAP.

Where the opposite side of the street does not have one of the street designations listed above, decorative lighting will be used. This is to ensure a continuous streetscape aesthetic on both sides of a street. All other streets, within the boundaries of the City Centre, as identified on Schedule A of the CCAP, will be guided by the standards set in the *Subdivision and Development Servicing Bylaw*.

Decorative Lighting Hardware

The recommended lighting hardware is the Philips Lumec Transit series TR-20 luminaires and poles with powder coat finish in semi-gloss textured black. All lighting temperature will be 4000K.

Tables 4 and **5** show the preferred configurations of decorative roadway and pedestrian luminaires and poles. Roadway lighting should be paired with pedestrian lighting wherever possible. The City may request additional elements on a case-by-case basis, as deemed appropriate by the General Manager Planning & Development.

Lighting standards will be guided by the standards set in the *Subdivision and Development Servicing Bylaw*, and other City standards and requirements.

Table 4 - Roadway and Pedestrian Lighting Configurations.

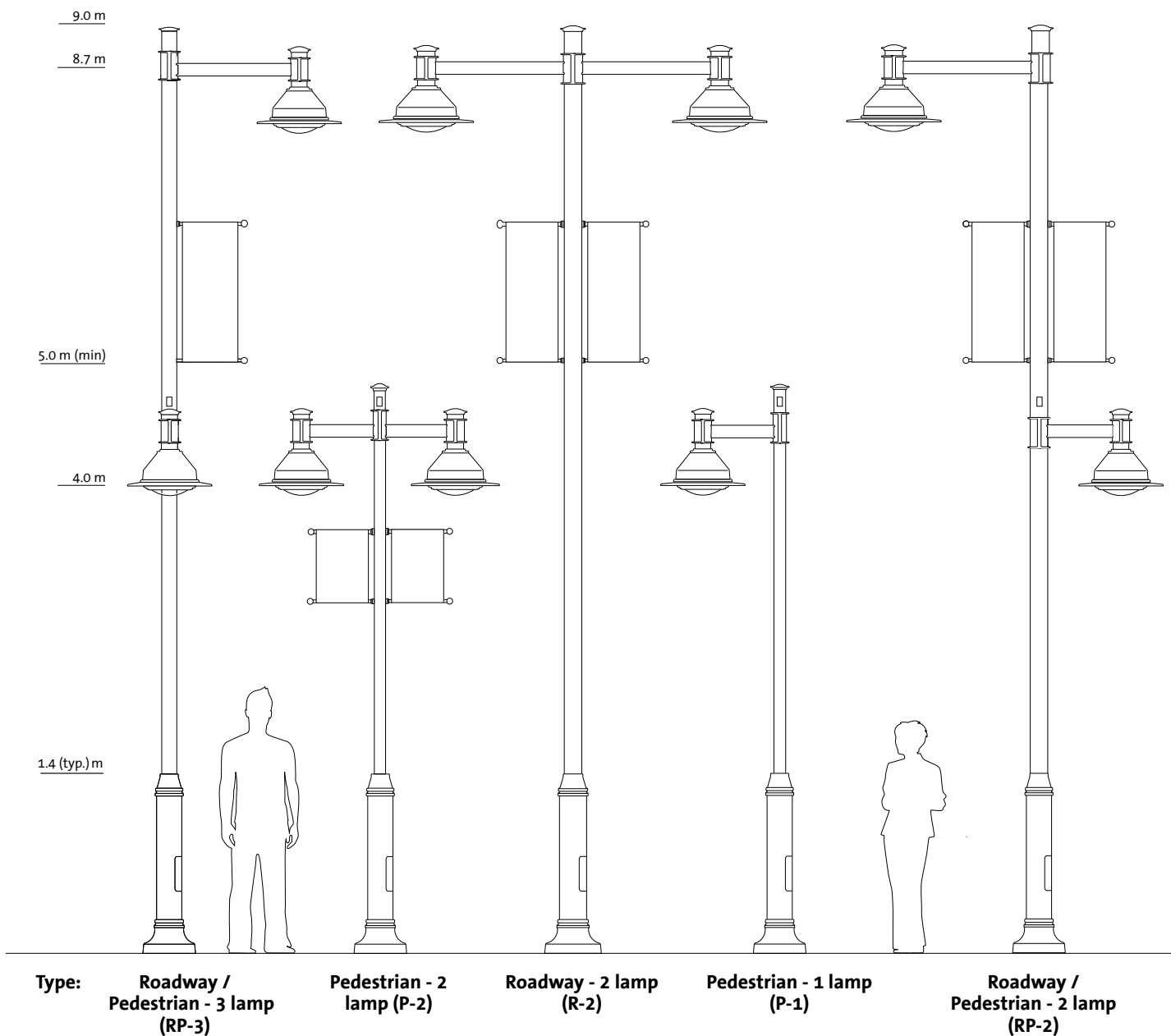
Mandatory Commercial Street Frontage	Secondary Active Street Frontage	Residential Street Frontage	Character Streets
RP-3 type luminaire at intersections with P-2 type luminaire along the Pedestrian Movement Zone .	Commercial Frontage RP-3 type luminaire at intersections with P-2 type luminaire along the Pedestrian Movement Zone . Residential Frontage RP-2 type luminaire at intersections with P-1 type luminaire along the Pedestrian Movement Zone .	RP-2 type luminaire at intersections with P-1 type luminaire along the Pedestrian Movement Zone .	Pinetree Way P-2 type luminaire along the Pedestrian Movement Zone . "Urban Boulevard" P-2 type luminaire along the Pedestrian Movement Zone . "Downtown Promenade" RP-3 type luminaire at intersections with P-2 type luminaire along the Pedestrian Movement Zone .

Notes:

1. All luminaire types along Mandatory Commercial Street Frontages and Character Streets will be equipped with exterior rated GFCI plugs to enable lighting installations and the necessary hardware and accessories for arm banners.
2. Where applicable, use R-2 type luminaire for the median.

Table 5 - Roadway and Pedestrian Lighting Specifications.

Feature	Specifications
Decorative Lighting	Manufacturer: Philips Lumec Transit series Luminaire: TR-20 Lamp Type: LED Mount: SN Colour: Semi-gloss textured black Finish: Powder coat Base: SM Accessories: See notes under Table 4.



6.4 Landscaping

Landscaping and street trees will help create an attractive public realm, contribute to a comfortable experience for pedestrians and micromobility users, and provide opportunities to interact with nature, while addressing sustainability goals and objectives such as enhancing biodiversity, reducing urban heat island effects and enhancing stormwater management.

Plant selection will need to provide high aesthetic value, be adaptable to climate change, be low maintenance and account for localized micro-climates. Landscaping and street trees will be guided by the standards set in the *Preferred Street Tree List* and *Tree Management Bylaw*, and shall comply with guidelines established by the City's Urban Forestry department. Tree planting standards are guided by the *Supplementary Specifications Master Municipal Construction Document*. The City of Coquitlam's *Maintenance of Boulevards Bylaw* should be referred to for details on maintenance.

Greenway Landscaping

Landscaping along greenways will enhance the public realm, enrich the biodiversity and foster a sense of place in City Centre. Plants will be drought-tolerant and low maintenance to ensure a clean look when mature.

Street Trees and Soil Cells

Street trees are a critical component to developing the character of an area. Tree species selection will be based on diversity, form and character to create a continuous tree canopy and emphasize the various street frontage types and Character Streets. Trees, high-quality soil to support mature growth (soil cells), lighting and underground utility placement must be coordinated to avoid conflict and achieve a minimum of 20m³ of soil volume per tree.

Stormwater Management and Rain Gardens

Rain gardens are concave landscaped areas where runoff from paving is captured, treated and detained before being infiltrated into the soil below. Rain gardens are designed to have an aesthetic appeal as well as a stormwater function with vegetation that helps to filter out contaminants.

Standard drawings and specifications for the design and construction of stormwater management and rain gardens are guided by the *Supplementary Specifications Master Municipal Construction Document*. Additional information is available in the *Coquitlam Green Development Guide* and *Metro Vancouver Stormwater Source Control Design Guidelines*.



Green infrastructure and landscaping to enrich the public realm and the biodiversity of the streetscape.



Green infrastructure and continuous street trees along the Pinetree Way Character Street and Citywide Greenway.

Building Frontage Zone Plantings

Landscaping within the **Furnishing Zone** along commercial frontages can be complemented with plantings in the **Building Frontage Zone**, creating a visual extension of greenery and continuity across the breadth of the public realm, from curb to the building face. Shrubs or tall grasses that block openness, transparency and visibility into the commercial/retail spaces should be avoided.

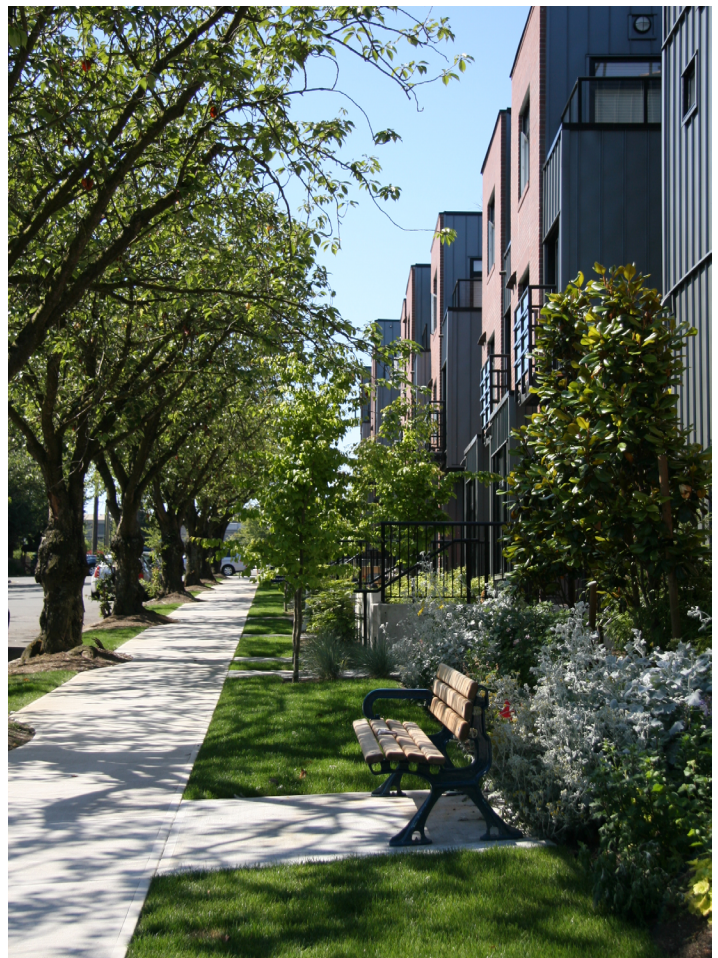
Planting and landscaping within the **Building Frontage Zone** of residential street frontages should extend out to the edge of the **Pedestrian Movement Zone**, provided there are no utility easement restrictions.

Buffer, Furnishing and Boulevard Zones Plantings

Select appropriate plantings to add seasonal colour, biodiversity and environmental benefit. Landscaping in these zones will help create a sense of place and define the public realm.



Landscaping in the Building Frontage Zone as an extension of the Furnishing Zone along a commercial fronting street.



Landscaping and a bench in the Building Frontage Zone to create a sense of place along a residential fronting street.

6.5 Public Art

Public art can serve as focal points in neighbourhoods and public spaces, contribute to a community's identity by adding character to the surrounding area, and enhance pedestrian interest in areas where people gather and socialize.

The *Public Art Policy & Project Plan* defines public art as art created by a professional artist and guides the review of public art along with associated guidelines to ensure that the art is of artistic excellence and high quality. Public artworks may be realized through a variety of media and can be permanent, temporary or rotating.

Throughout City Centre, public art will be located in prominent and highly visible locations. Applicants are encouraged to explore public art design elements that provide opportunities for play and exploration, and draw interest.

Public art is encouraged as part of the following, in accordance with the *Public Art Policy and Project Plan* and Part 4 of the *Citywide Official Community Plan*:

- All new development;
- Greenways as identified on **Figure 11** and Schedule G of the CCAP;
- Linear Park as identified on **Figures 11** and **12** and Schedule E and G of the CCAP;
- The three Precincts as identified in Section 6.1 of the CCAP; and
- Privately-Owned Publicly-Accessible Open Spaces, including Gateways as identified on Schedule J of the CCAP.

Public safety is a universal criteria for all types of public art. The public art installation will be set back from the roadway to avoid blocking vehicular sightlines and the risk of being hit by vehicles. The public art feature will not have sharp, protruding, or overhanging edges that can trip or catch a passerby. In addition, features and associated lighting will not be a distraction to drivers, pedestrians and micromobility users.

To ensure safety, the installation and location of public art shall be reviewed and approved by the General Manager of Engineering & Public Works, in accordance with the *Public Art Policy and Project Plan*.



"Gathering Salmon" Painted by Elham Sarvi and sculpture designed by Jody Broomfield. Sarvi's design reflects the cultural diversity of Coquitlam, using motifs inspired by the Korean, Chinese and Iranian cultures to promote peace and harmony.

6.6 Wayfinding

Wayfinding signage help connect pedestrians, micromobility and transit users to major venues, facilities and activity centres. They also allow individuals easily enter a neighbourhood and navigate local streets by effortlessly finding their destination.

Wayfinding signage will be provided in key areas to enhance and support multi-modal travel throughout City Centre in accordance with the *City Wayfinding Plan*. **Figure 17** provides an overview of the types of wayfinding signage for pedestrians, micromobility, and transit and motor-vehicle users.



Wayfinding signage outside the Lafarge Lake–Douglas SkyTrain station.

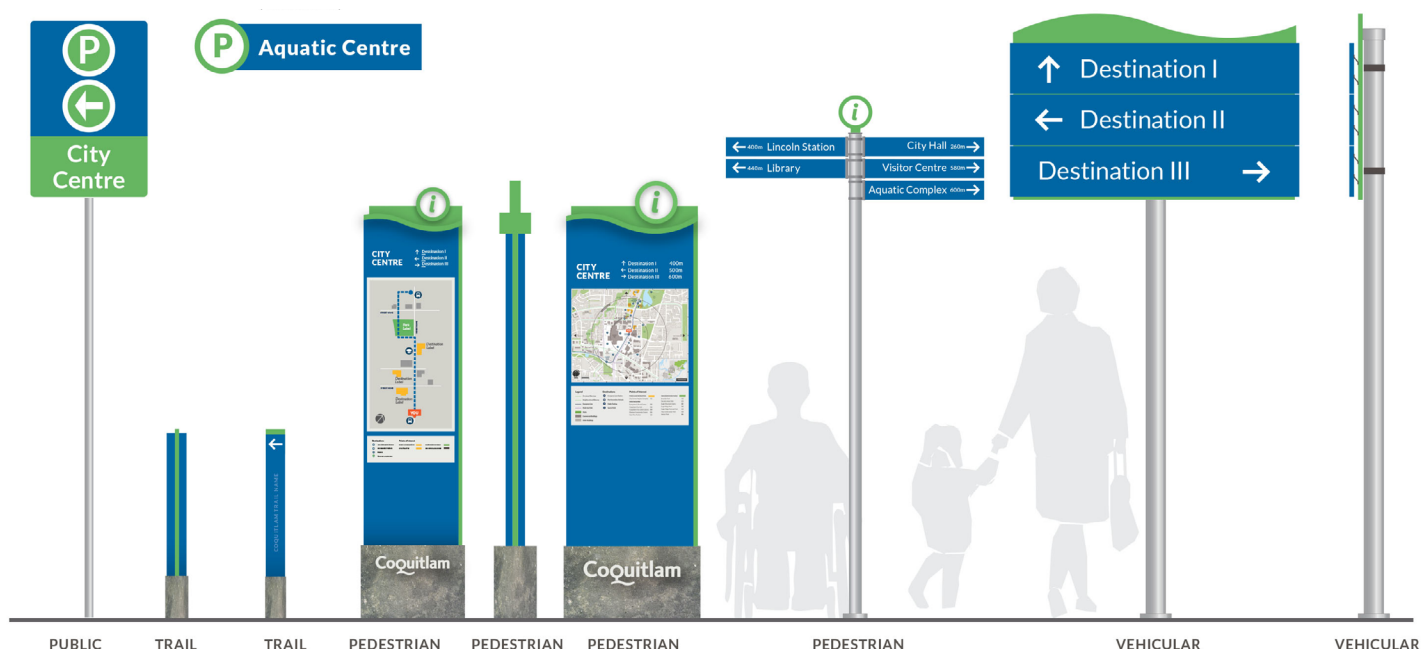


Figure 17 - Examples of Wayfinding Signage and Street Markers.

7.0 IMPLEMENTATION



Coquitlam City Hall.

7.1 Implementation

Over the next 25 years, the CCAP will guide future growth and continue to transform City Centre into a vibrant downtown for residents and businesses of Coquitlam that is an inclusive and happy place to live, learn, work and play. The CCSG is a companion document intended to ensure that as growth occurs, the public realm, streets and public spaces are designed to support City Centre's role as the downtown and heart of Coquitlam and the northeast region. To reflect the cultural diversity of Coquitlam, applicants should explore opportunities to work with the community and the *kʷikwə́lam* First Nation on the design of the public realm.

The following implementation actions are required to ensure the broader vision of the CCSG is achieved.

Replacement of Older Streetscape Furniture

To ensure a continuous streetscape in City Centre, in areas where the street furniture is that of a previous design guideline, explore opportunities through frontage improvements as part of redevelopment, maintenance and capital improvements or replacement projects, that are consistent with the CCSG, the *Subdivision and Development Servicing Bylaw*, and other City standards and requirement.

Update to the City Centre Area Plan

Update the CCAP to increase transportation options in City Centre by introducing the concept of micromobility. The CCAP envisions a network of dedicated micromobility facilities (infrastructure and supporting amenities) that provide for safe and convenient biking and rolling that reduces conflicts with buses, trucks and automobiles. Through the upcoming update of the STP and development of the E-Mobility Strategy, the City will develop policies, standards, bylaws, and educational materials to seamlessly integrate micromobility into the streetscape.

Streetscape Enhancement Management Committee

The cross-departmental Streetscape Enhancement Management Committee will focus on establishing new standards/guidelines, improving processes for identification and delivery of amenities and programmed features such as lighting, seating, bike racks, and garbage and recycling receptacles, and facilitating implementation.

Detailed Drawings for the Supplementary Specifications Master Municipal Construction Document

Update the *Supplementary Specifications Master Municipal Construction Document* and other City standards to include technical drawings and details on the installation of green infrastructure, paver on concrete and street furniture on pavers.

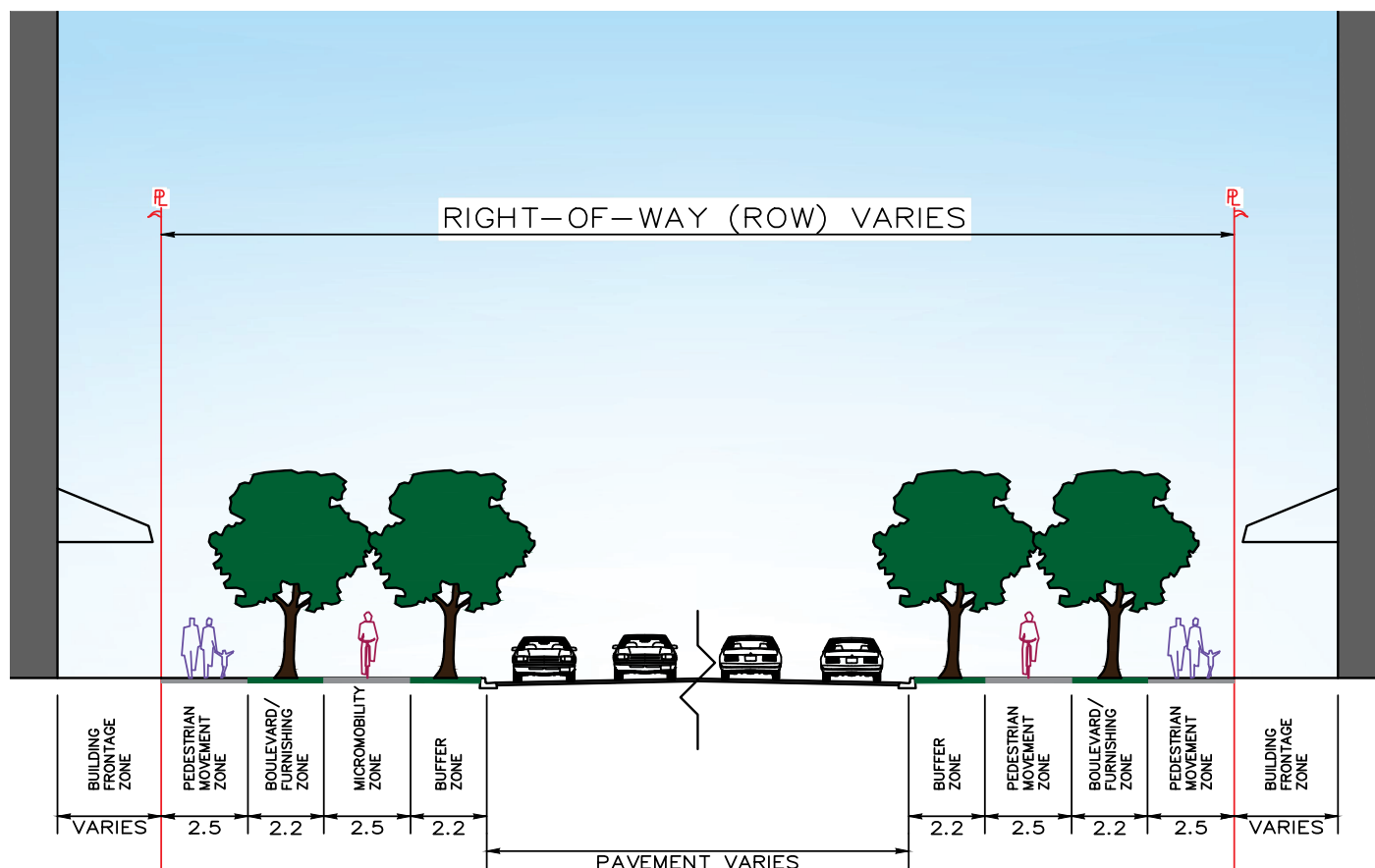


Unique Coquitlam horticultural display replicating the famous Chief of the Undersea World sculpture by Bill Reid.

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APPENDIX A:

STREET CROSS-SECTIONS



NOTE:

1. THE CROSS-SECTIONS IN APPENDIX A REPRESENT TYPICAL MID-BLOCK SECTIONS. ADDITIONAL ROW MAY BE REQUIRED, DEPENDING ON SITE-SPECIFIC REQUIREMENTS (E.G. ROAD WIDENING FOR AUXILIARY LANES AT INTERSECTIONS).

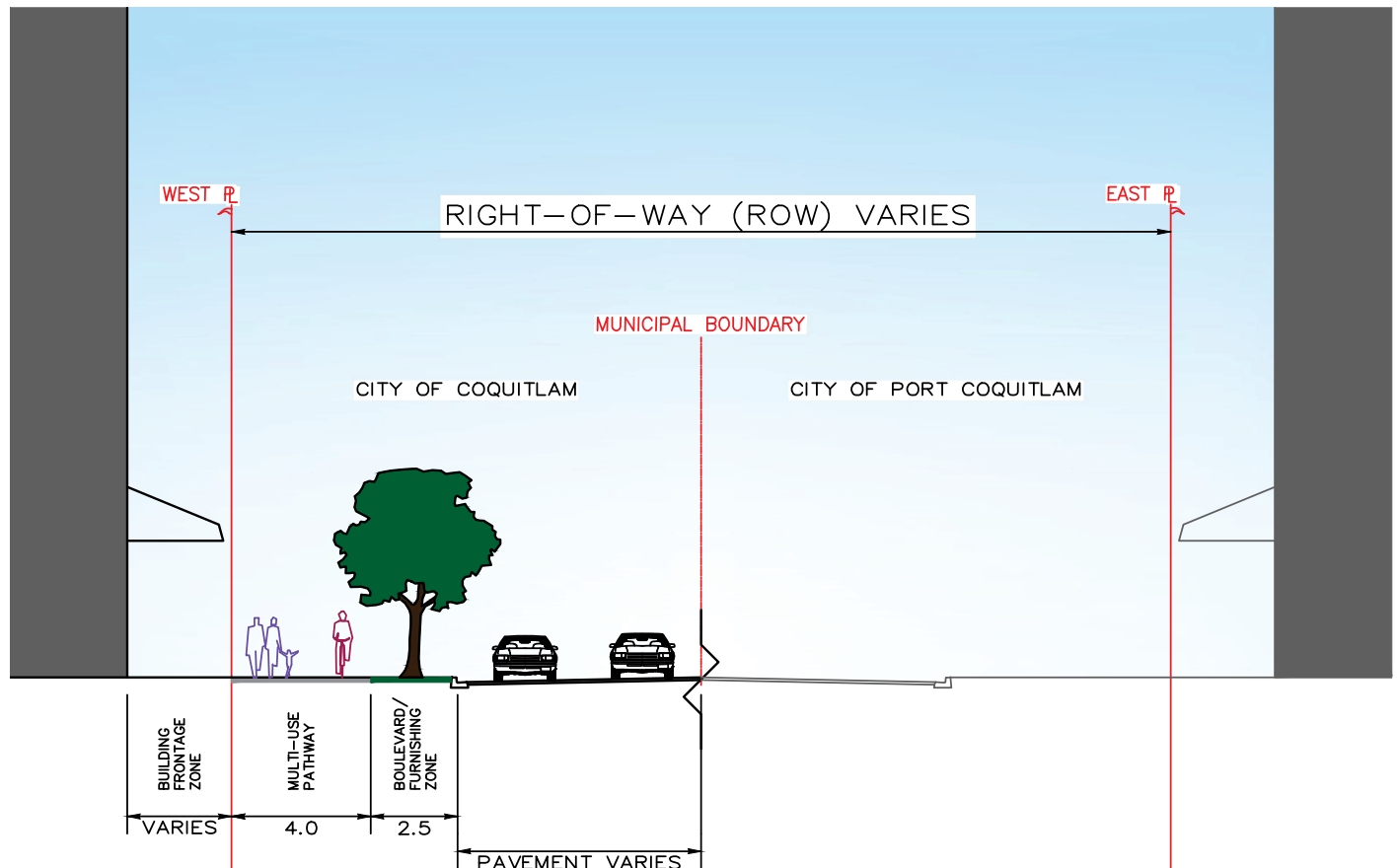
PLOTTED: 10-May-22

LOUGHEED HWY
BARNET HWY
PINETREE WAY
JOHNSON ST

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

DRAWING NUMBER:

SECTION 1



NOTE:

1. THE MULTI-USE PATHWAY ON WESTWOOD STREET TRANSITIONS FROM THE WEST SIDE (SOUTH OF LINCOLN AVENUE) TO THE EAST SIDE (NORTH OF LINCOLN AVENUE).
2. THE CROSS-SECTIONS IN APPENDIX A REPRESENT TYPICAL MID-BLOCK SECTIONS. ADDITIONAL ROW MAY BE REQUIRED, DEPENDING ON SITE-SPECIFIC REQUIREMENTS (E.G. ROAD WIDENING FOR AUXILIARY LANES AT INTERSECTIONS).

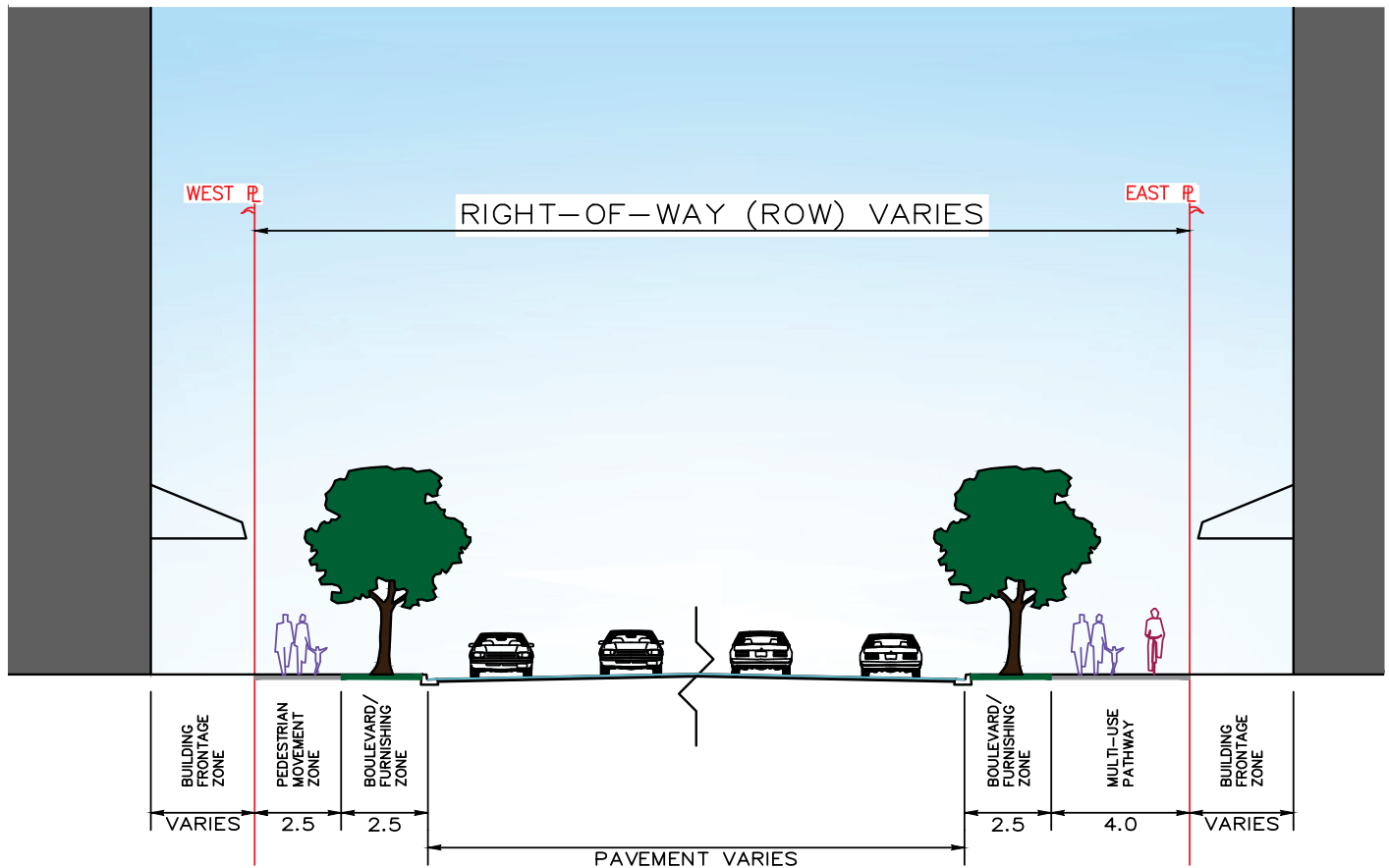
PLOTTED: 10-May-22

WESTWOOD ST (SOUTH OF LINCOLN AVE)

DATE:	MAY/2022
DRAWN:	JA
SCALE:	N.T.S.

DRAWING NUMBER:

SECTION 2



NOTE:

1. THE MULTI-USE PATHWAY ON WESTWOOD STREET TRANSITIONS FROM THE WEST SIDE (SOUTH OF LINCOLN AVENUE) TO THE EAST SIDE (NORTH OF LINCOLN AVENUE).
2. THE CROSS-SECTIONS IN APPENDIX A REPRESENT TYPICAL MID-BLOCK SECTIONS. ADDITIONAL ROW MAY BE REQUIRED, DEPENDING ON SITE-SPECIFIC REQUIREMENTS (E.G. ROAD WIDENING FOR AUXILIARY LANES AT INTERSECTIONS).

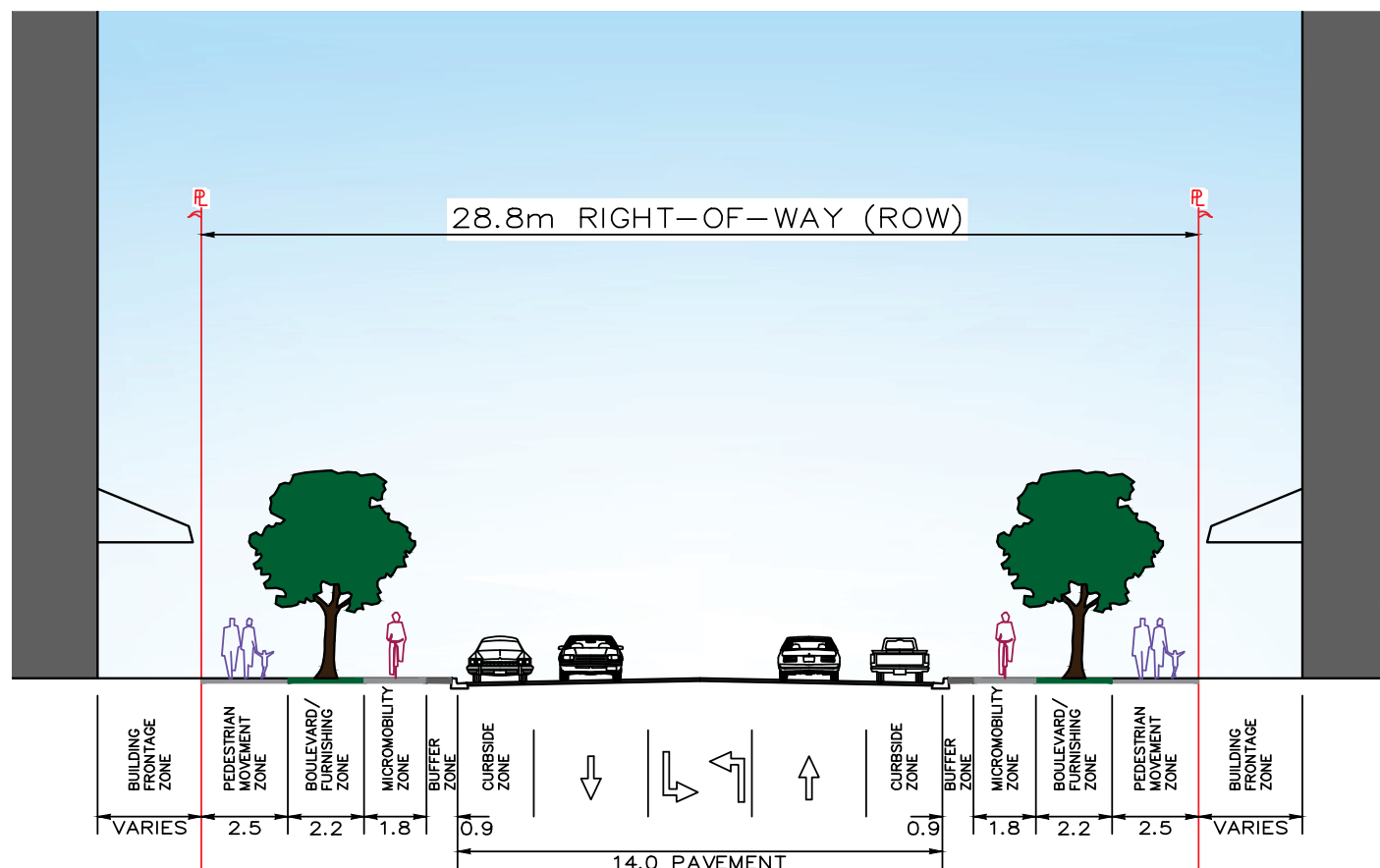
PLOTTED: 10-May-22

WESTWOOD ST (NORTH OF LINCOLN AVE)

DATE:	MAY/2022
DRAWN:	JA
SCALE:	N.T.S.

DRAWING NUMBER:

SECTION 3



NOTE:

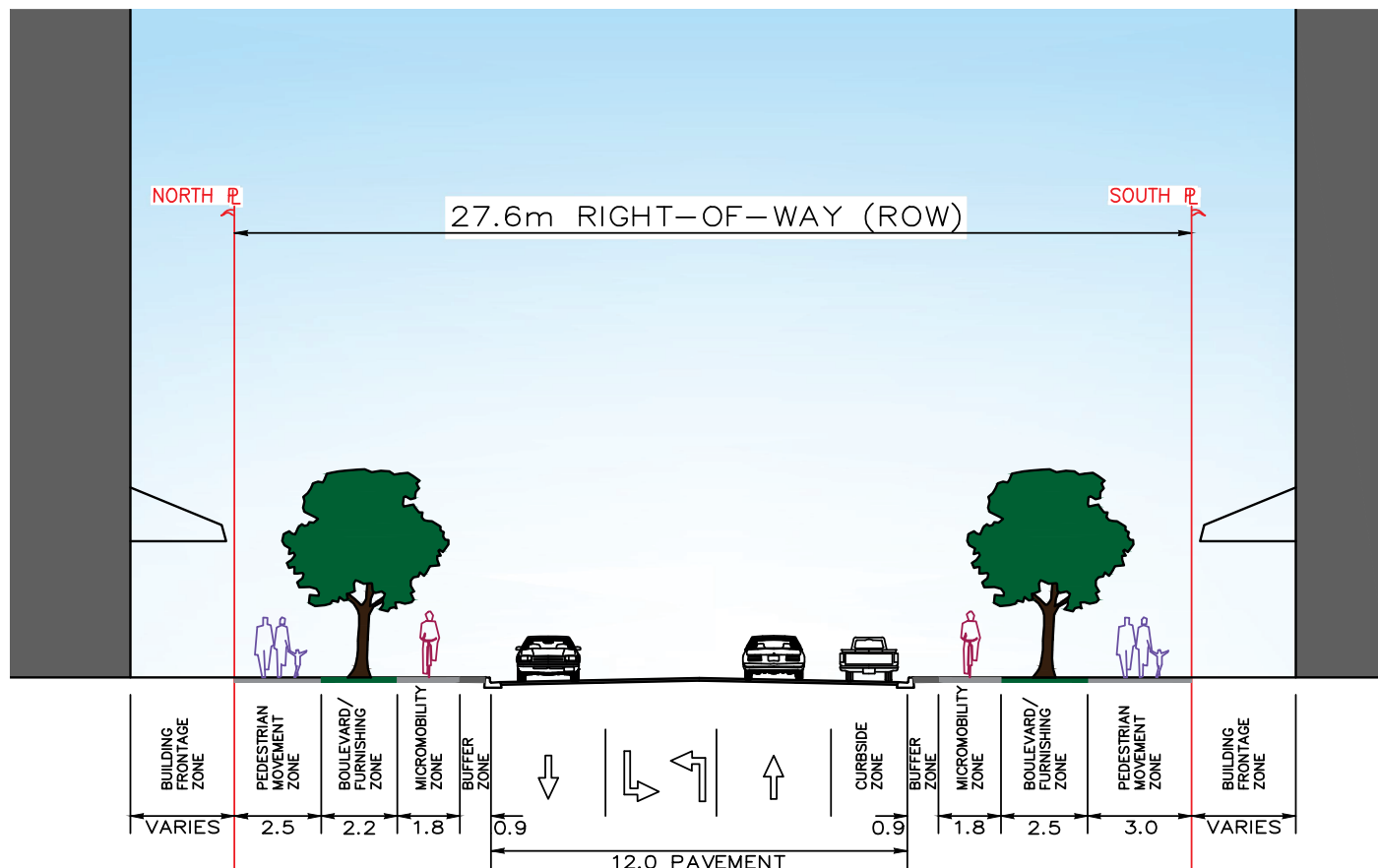
1. LINCOLN AVENUE (EAST OF PINETREE WAY) IS CLASSIFIED ARTERIAL AND REQUIRES ADDITIONAL PAVEMENT (+18M EXISTING PAVEMENT WIDTH), WHICH IN TURN IMPACTS DEDICATION REQUIREMENTS. OTHER STREETSCAPE ELEMENTS (PEDESTRIAN MOVEMENT ZONE, BOULEVARD/FURNISHING ZONE, ETC.) WILL BE CONSISTENT ACROSS ALL OF LINCOLN AVENUE.
2. CURBSIDE ZONES REFERS TO A SHARED SPACE FOR PARKING, BIKE-SHARING, MOBILITY HUBS, PARKLETS, PICK-UP AND DROP-OFF, LOADING, ETC. CURBSIDE ZONES AS SHOWN ARE CONCEPTUAL, AND THEIR EXACT LOCATION AND FUNCTION WILL BE SITE-SPECIFIC. CURBSIDE USES WILL NEED TO BE RESTRICTED ALONG SOME PORTIONS OF ROAD.
3. THE CROSS-SECTIONS IN APPENDIX A REPRESENT TYPICAL MID-BLOCK SECTIONS. ADDITIONAL ROW MAY BE REQUIRED, DEPENDING ON SITE-SPECIFIC REQUIREMENTS (E.G. ROAD WIDENING FOR AUXILIARY LANES AT INTERSECTIONS).

PLOTTED: 10-May-22

GLEN DR
PACIFIC ST
LINCOLN AVE (WEST OF PINETREE WAY)
PONDEROSA ST
NEW COLLECTOR ROAD (ROAD A)

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

DRAWING NUMBER:
SECTION 4



NOTE:

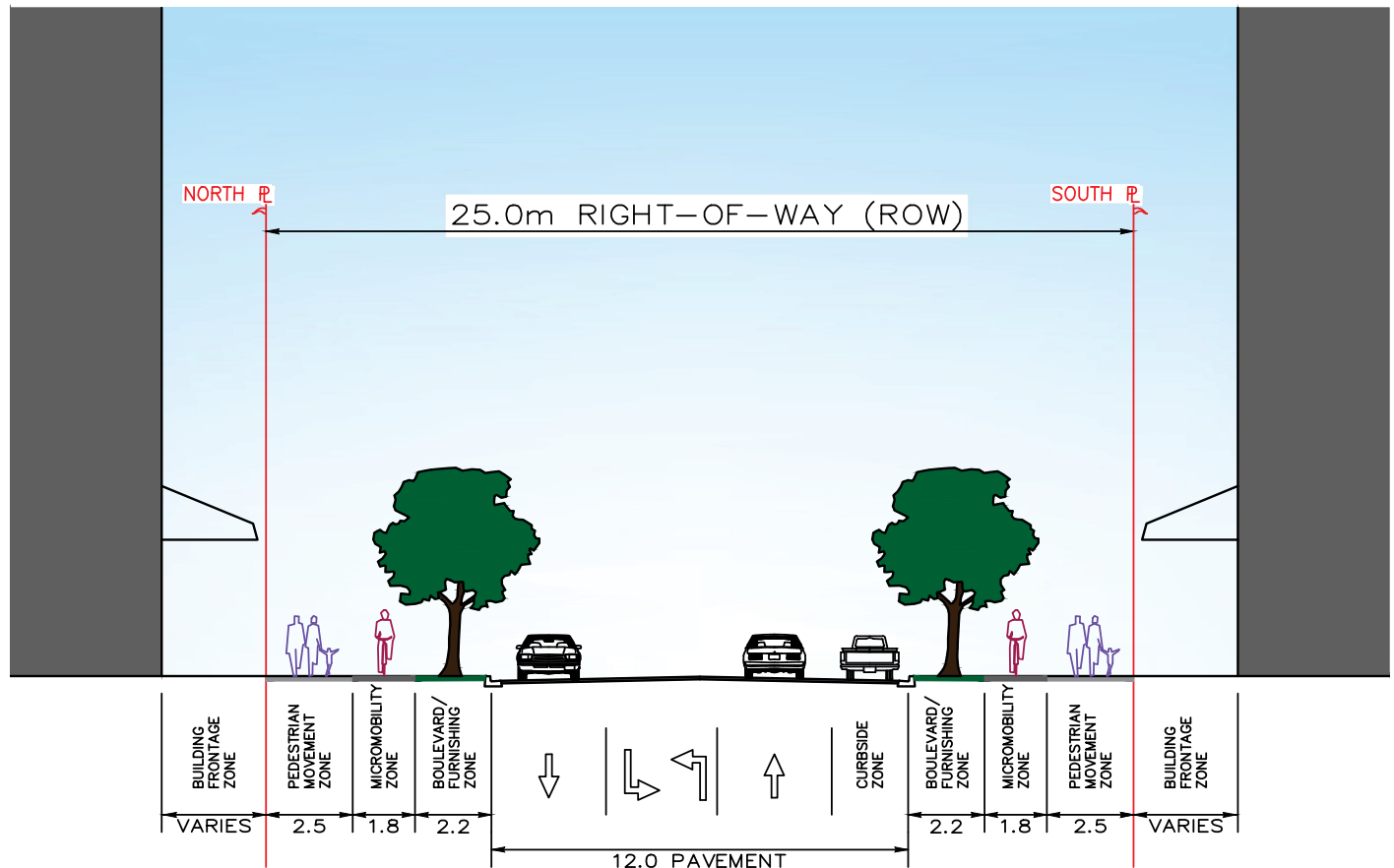
1. CURBSIDE ZONES REFERS TO A SHARED SPACE FOR PARKING, BIKE-SHARING, MOBILITY HUBS, PARKLETS, PICK-UP AND DROP-OFF, LOADING, ETC. CURBSIDE ZONES AS SHOWN ARE CONCEPTUAL, AND THEIR EXACT LOCATION AND FUNCTION WILL BE SITE-SPECIFIC. CURBSIDE USES WILL NEED TO BE RESTRICTED ALONG SOME PORTIONS OF ROAD.
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PLOTTED: 10-May-22

NORTHERN AVE

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

DRAWING NUMBER:
SECTION 5



NOTE:

1. CURBSIDE ZONES REFERS TO A SHARED SPACE FOR PARKING, BIKE-SHARING, MOBILITY HUBS, PARKLETS, PICK-UP AND DROP-OFF, LOADING, ETC. CURBSIDE ZONES AS SHOWN ARE CONCEPTUAL, AND THEIR EXACT LOCATION AND FUNCTION WILL BE SITE-SPECIFIC. CURBSIDE USES WILL NEED TO BE RESTRICTED ALONG SOME PORTIONS OF ROAD.
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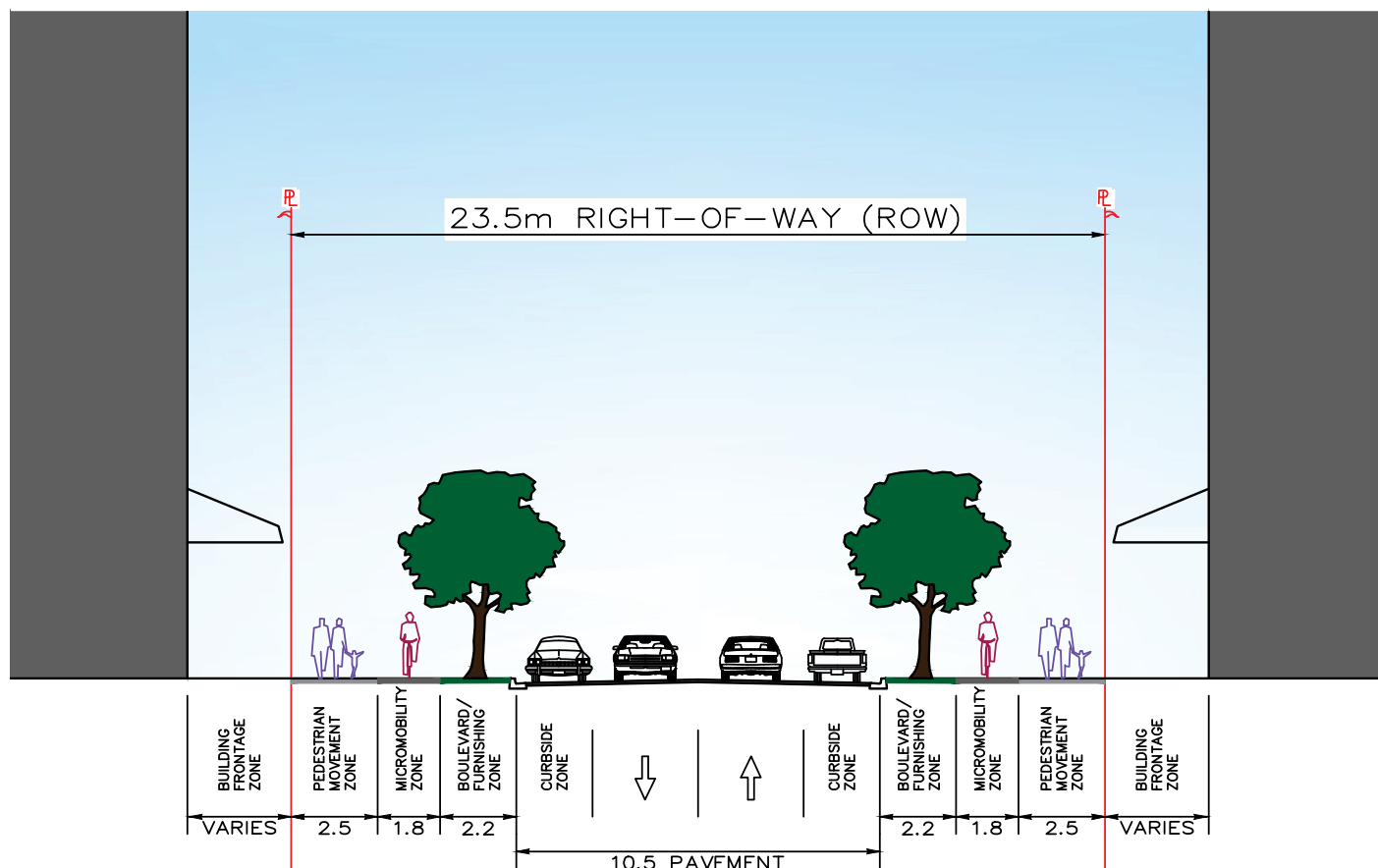
PLOTTED: 10-May-22

ANSON AVE

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

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SECTION 6



NOTE:

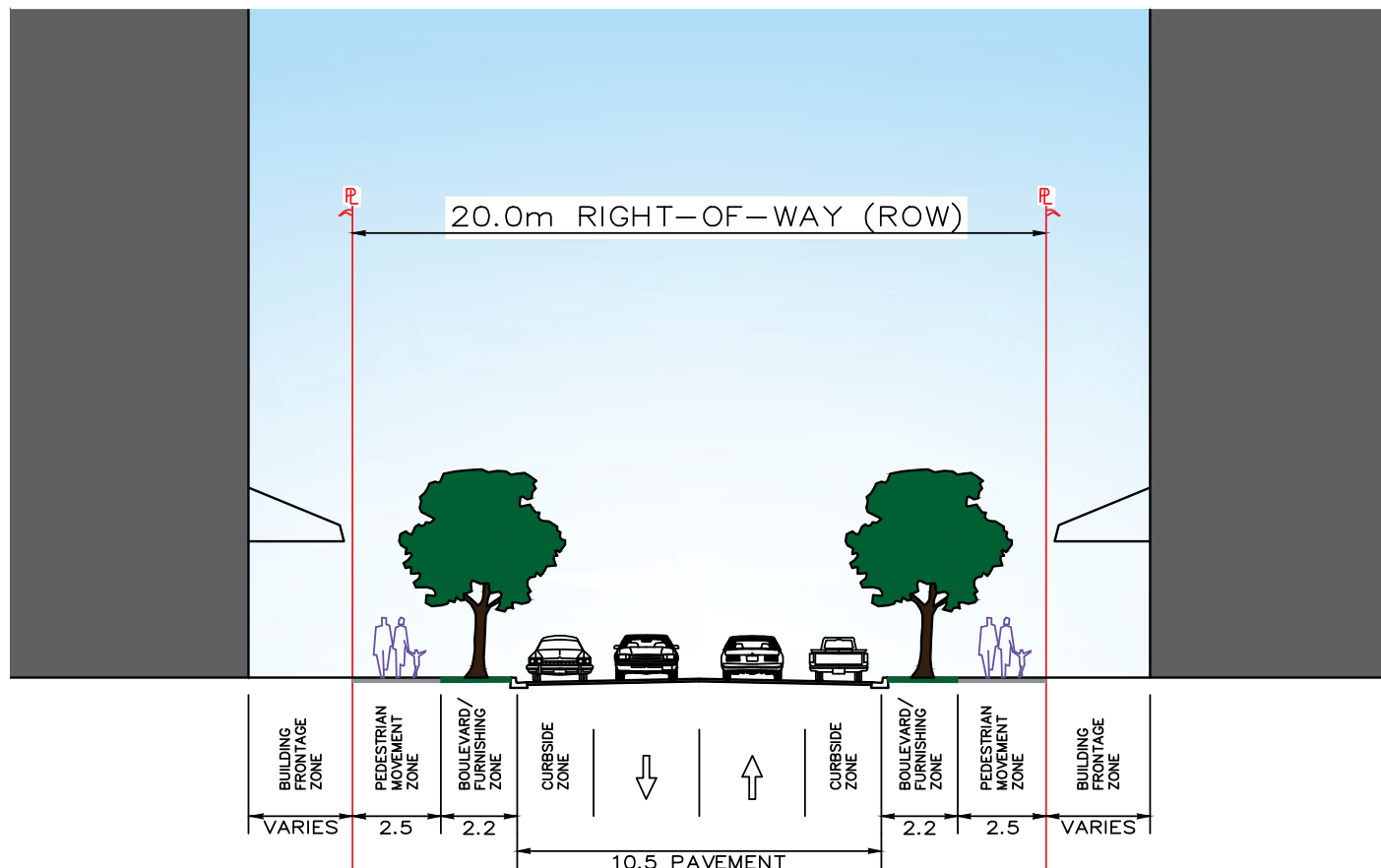
1. CURBSIDE ZONES REFERS TO A SHARED SPACE FOR PARKING, BIKE-SHARING, MOBILITY HUBS, PARKLETS, PICK-UP AND DROP-OFF, LOADING, ETC. CURBSIDE ZONES AS SHOWN ARE CONCEPTUAL, AND THEIR EXACT LOCATION AND FUNCTION WILL BE SITE-SPECIFIC. CURBSIDE USES WILL NEED TO BE RESTRICTED ALONG SOME PORTIONS OF ROAD.
2. THE CROSS-SECTIONS IN APPENDIX A REPRESENT TYPICAL MID-BLOCK SECTIONS. ADDITIONAL ROW MAY BE REQUIRED, DEPENDING ON SITE-SPECIFIC REQUIREMENTS (EG. ROAD WIDENING FOR AUXILIARY LANES AT INTERSECTIONS).

PLOTTED: 10-May-22

BALDWIN ST EAST
NEW LOCAL ROAD (ROAD B)

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

DRAWING NUMBER:
SECTION 7



NOTE:

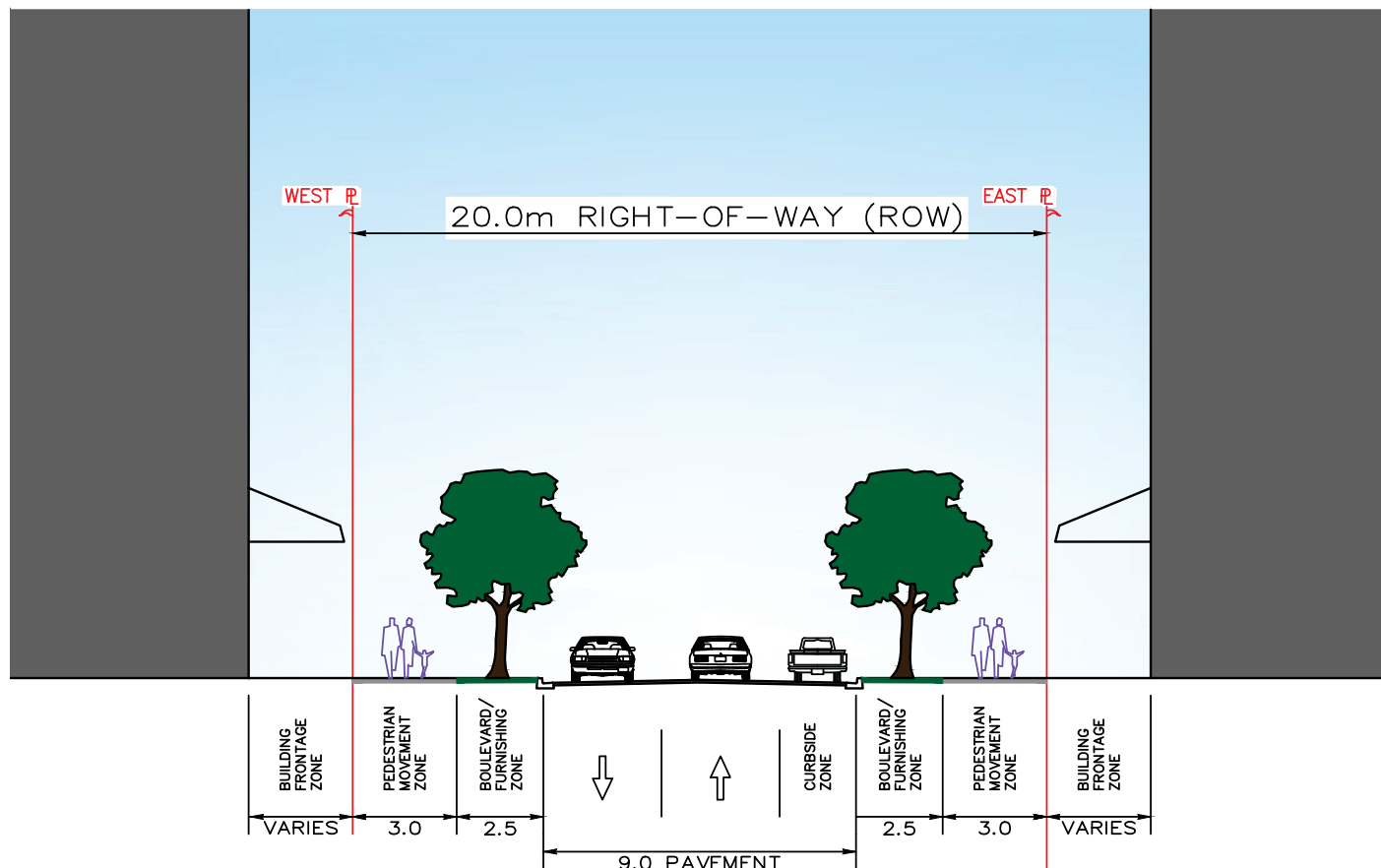
1. CURBSIDE ZONES REFERS TO A SHARED SPACE FOR PARKING, BIKE-SHARING, MOBILITY HUBS, PARKLETS, PICK-UP AND DROP-OFF, LOADING, ETC. CURBSIDE ZONES AS SHOWN ARE CONCEPTUAL, AND THEIR EXACT LOCATION AND FUNCTION WILL BE SITE-SPECIFIC. CURBSIDE USES WILL NEED TO BE RESTRICTED ALONG SOME PORTIONS OF ROAD.
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PLOTTED: 10-May-22

ATLANTIC AVE
BALDWIN ST WEST

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

DRAWING NUMBER:
SECTION 8



NOTE:

1. APPLICANTS ARE ENCOURAGED TO PURSUE UNIQUE, INNOVATIVE AND HIGH-QUALITY STREETSCAPE DESIGNS, AS OUTLINED IN SECTION 3.1.5 "DOWNTOWN PROMENADE" CHARACTER STREET.
2. CURBSIDE ZONES REFERS TO A SHARED SPACE FOR PARKING, BIKE-SHARING, MOBILITY HUBS, PARKLETS, PICK-UP AND DROP-OFF, LOADING, ETC. CURBSIDE ZONES AS SHOWN ARE CONCEPTUAL, AND THEIR EXACT LOCATION AND FUNCTION WILL BE SITE-SPECIFIC. CURBSIDE USES WILL NEED TO BE RESTRICTED ALONG SOME PORTIONS OF ROAD.
3. THE CROSS-SECTIONS IN APPENDIX A REPRESENT TYPICAL MID-BLOCK SECTIONS. ADDITIONAL ROW MAY BE REQUIRED, DEPENDING ON SITE-SPECIFIC REQUIREMENTS (E.G. ROAD WIDENING FOR AUXILIARY LANES AT INTERSECTIONS).

PLOTTED: 10-May-22

THE HIGH ST

DATE: MAY/2022
DRAWN: JA
SCALE: N.T.S.

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SECTION 9

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Planning and Development

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